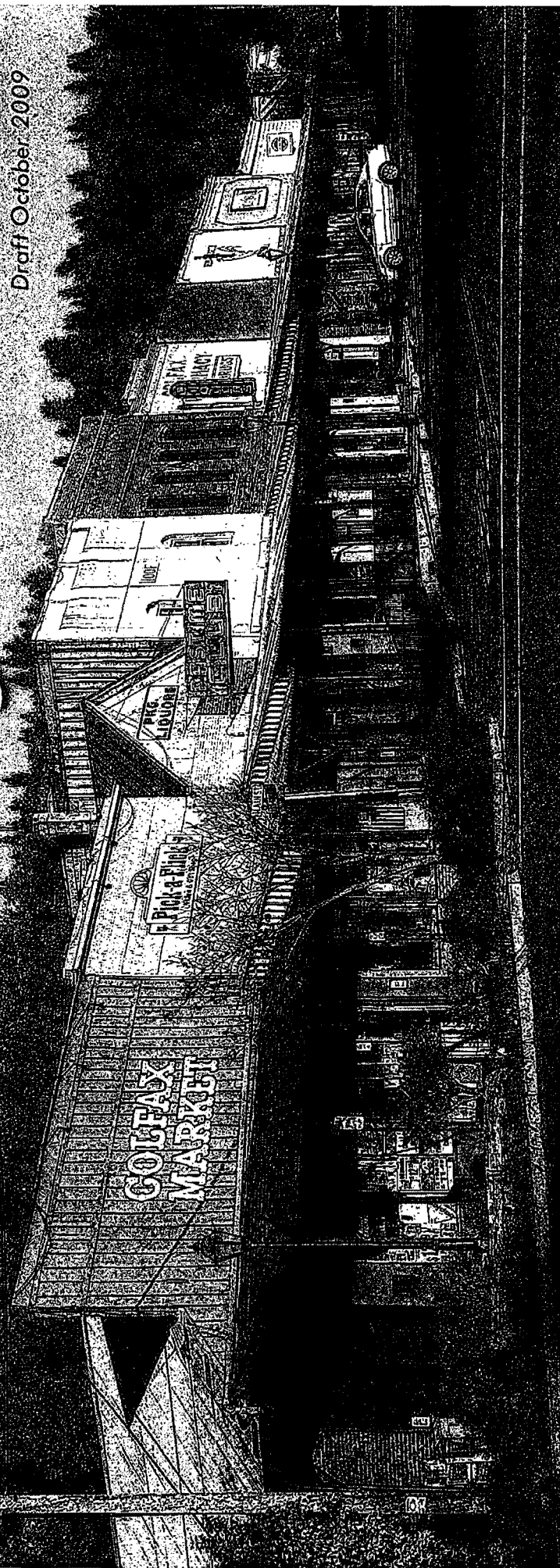


City of Colfax

# Historic District Design Master Plan

Draft October 2009



Prepared by:  
880 Main Drive, Suite 100  
Reno, CA 95769

CITY OF COLFAX

**City of Colfax**

# **Historic District Design Master Plan**

October 2009

Prepared for:  
City of Colfax



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ENVIRONMENTAL CONSULTING • PLANNING • LANDSCAPE ARCHITECTURE

# ACKNOWLEDGEMENTS

## **City of Colfax Staff**

Gary W. Price, Planning Director

## **Ad-Hoc Steering Committee**

David Ackerman

Ken Delfino

Carol Dotto

Lynn Tausch

Chris Toepfer

Roger Staab

James Wood

## **Citizens**

Gunnar & Ann Marie Henrioulle

Conny Hyman

Susan Johnson

Rebecca Martinez

Terry Murch

Bili Winter

## **Consultant Staff**

Kate Kirsh, RLA, ISA

Bart van der Zeewu, RLA, LEED AP

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- 2. Part A: City of Colfax Historic District Design Guidelines**
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# 1. INTRODUCTION

## 1.1 Master Plan Overview

The Colfax Historic District Master Plan comprises three major components which collectively provide guidance for the future economic development of the City of Colfax Historic District. These are:

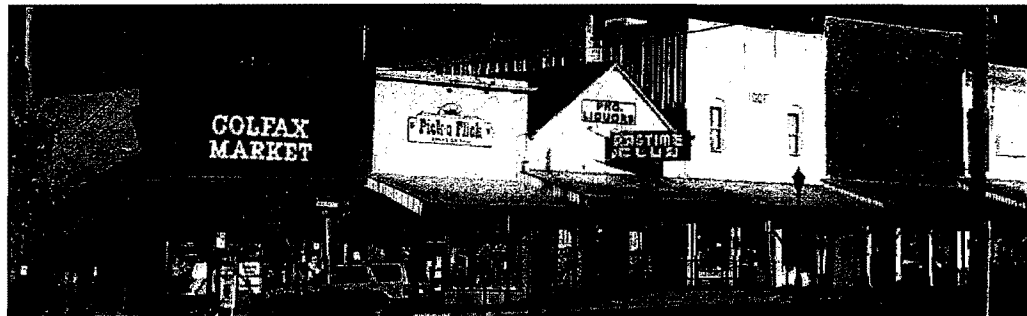
**PART A – The Historic District Design Guidelines** expand upon the direction provided by the City of Colfax General Plan, Municipal Code, ordinances, and Design Guidelines to provide more detailed guidance on improvements to properties and structures in the Historic District.

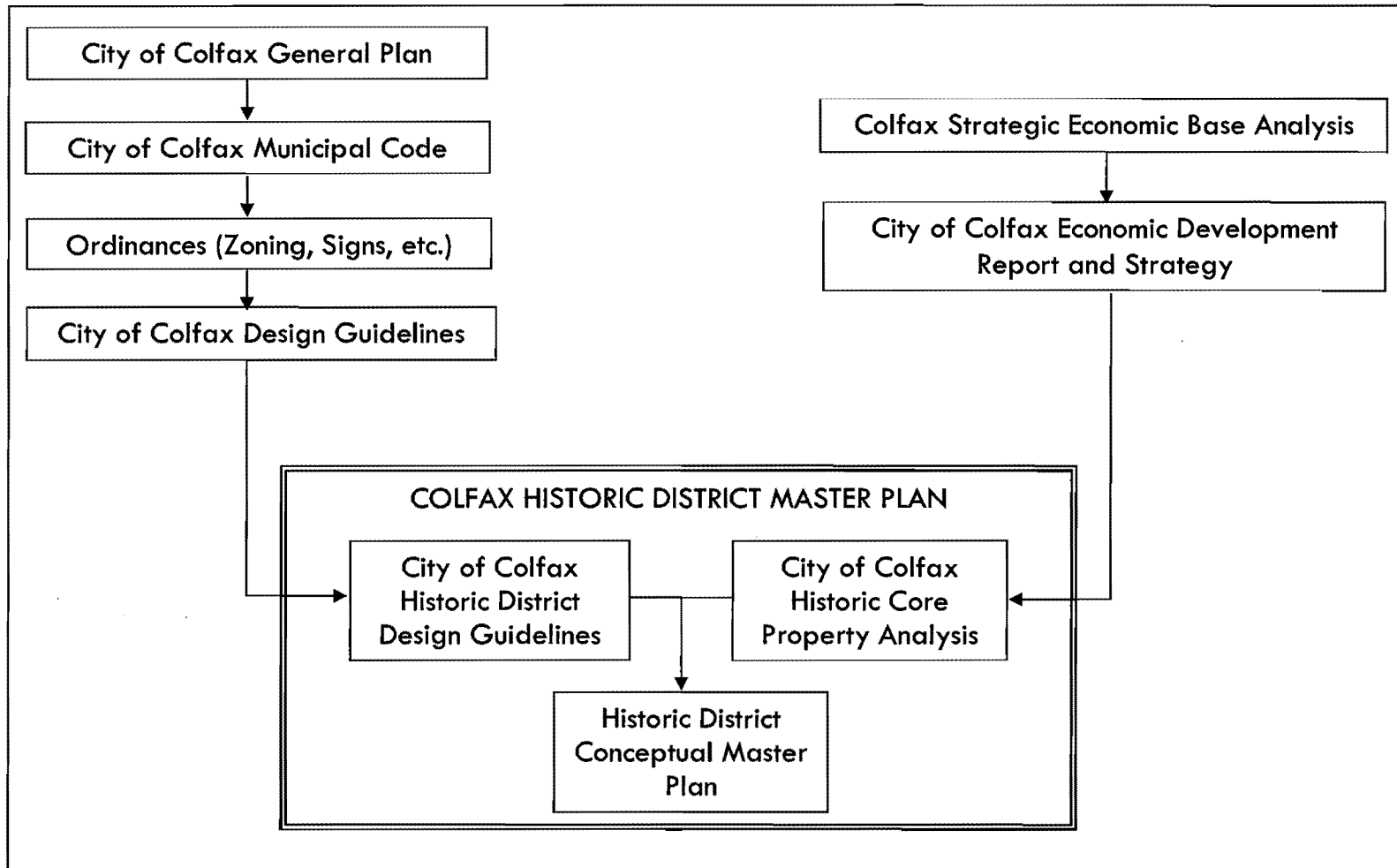
**PART B – The Historic Core Property Analysis** focuses on the heart of the Historic District because revitalization of this area is critical to economic stimulation of the entire Historic District. The Historic Core Property Analysis provides an inventory of properties and analysis of land uses and streetscape conditions in the Historic Core. It also provides an evaluation of the economic strengths and challenges, as well as recommendations for business mix, job creation, and economic development consistent with the findings of the City of Colfax Economic Development Report and Strategy.

**PART C – The Historic District Conceptual Master Plan** includes detailed recommendations for streetscape and public open space improvements to the Historic Core, connections and gateways throughout the Historic District, a Phasing Plan for implementation of the Historic Core improvements, cost estimates for each of the five separate phases, and funding options.

## 1.2 Planning and Regulatory Context

The direction provided by the Colfax Historic District Master Plan derives from and further refines guidance provided by several other comprehensive planning and regulatory vehicles previously adopted by the City of Colfax. The relationship between the Colfax Historic District Master Plan and these other resources is illustrated in Figure 1. The Colfax Historic District Master Plan is not intended to be a substitute or replacement for any of the direction or requirements specified in these other resources.





**Figure 1 – Planning and Regulatory Context for the Historic District Master Plan**

### *1.3 Implementation Approach*

Implementation of the Colfax Historic District Master Plan will be most successful if all three parts are simultaneously pursued. Each part synergistically supports and reinforces the others for the maximum economic vitality of the Historic District.

Recommendations in the **Historic Core Property Analysis** include diversification of the business mix with an emphasis specific business sectors. These include Niche Retail, Local Products, Destination Hotel/Conference Center, Bed & Breakfast Accommodations, and Outdoor Recreation. Development of opportunities to promote Colfax as a destination are also recommended, such as a Railroad Museum/Transportation Center, an Outdoor Recreation Resource Center, and hosting more special events and festivals with a regional appeal.

The analysis also suggests the City pursue Economic Gardening as a strategy for job creation in concert with the four key components that the City of Colfax Economic Development Report and Strategy identifies as required to build the success of the City overall as a tourist destination. The first component is organization of community businesses and local government to create a vision and theme for the City of Colfax image. Promotion and marketing are identified as the second component to enhance the name recognition and image of Colfax as a destination for tourism. A comprehensive Marketing Plan should be formulated as part of this component. The third essential component is an urban design strategy. The improvements suggested in the Historic District Conceptual Master Plan are a major step towards implementing this strategy. The final component needed to foster a successful tourist destination identity for Colfax is the development of specific tourist oriented 'products' that will draw people to the City, such as events, festivals, accommodations, dining, goods, services, and leisure activities. The Historic Core Property Analysis offers specific suggestions for development of such products.

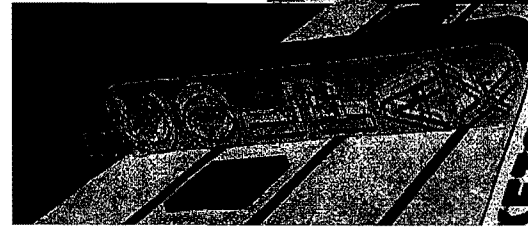
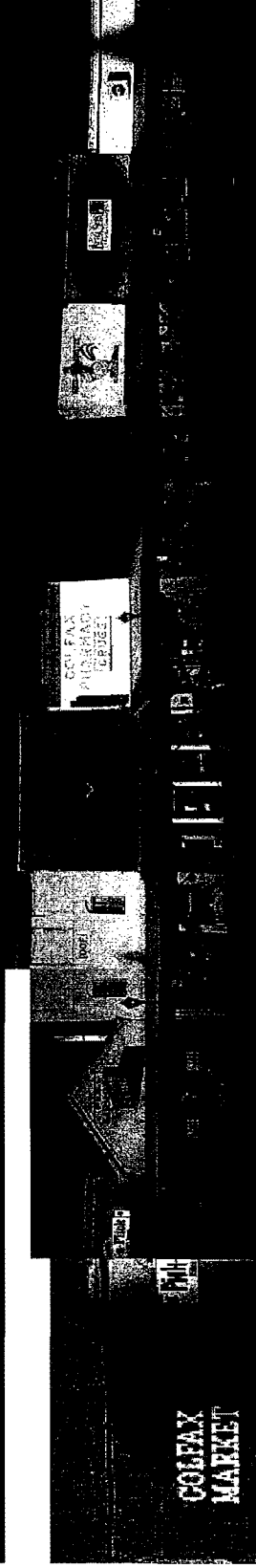
Phased implementation of the streetscape and public open space improvements included in the **Historic District Conceptual Master Plan** will be required to minimize disruption to local businesses and optimize available funding. The first phase focuses on the main approach to the Historic along Grass Valley Street. The second phase includes the section of North Main Street from the Grass Valley Street intersection to the north boundary of the Historic Core. Phase three includes the area along South Main Street to Church Street. Phase four addresses public open space improvements in the area east of the historic Freight Depot building. The final phase includes the existing pocket park adjacent to the library and additional parking with a pedestrian connection on the east side of South Main Street.

Finally, ongoing compliance with the **Historic District Design Guidelines** by new construction and renovation will be required to maintain the historic character of the district as a key element of economic revitalization while other improvements and strategies are being implemented. Implementation of the Historic District Master Plan will need the City's commitment to the long term, strategic vision while completing the many tactical initiatives describe above. It will also depend upon successful collaboration and consensus among diverse groups of citizens and merchants for the future benefit of the larger community .

# **Part A: City of Colfax Historic District Design Guidelines**



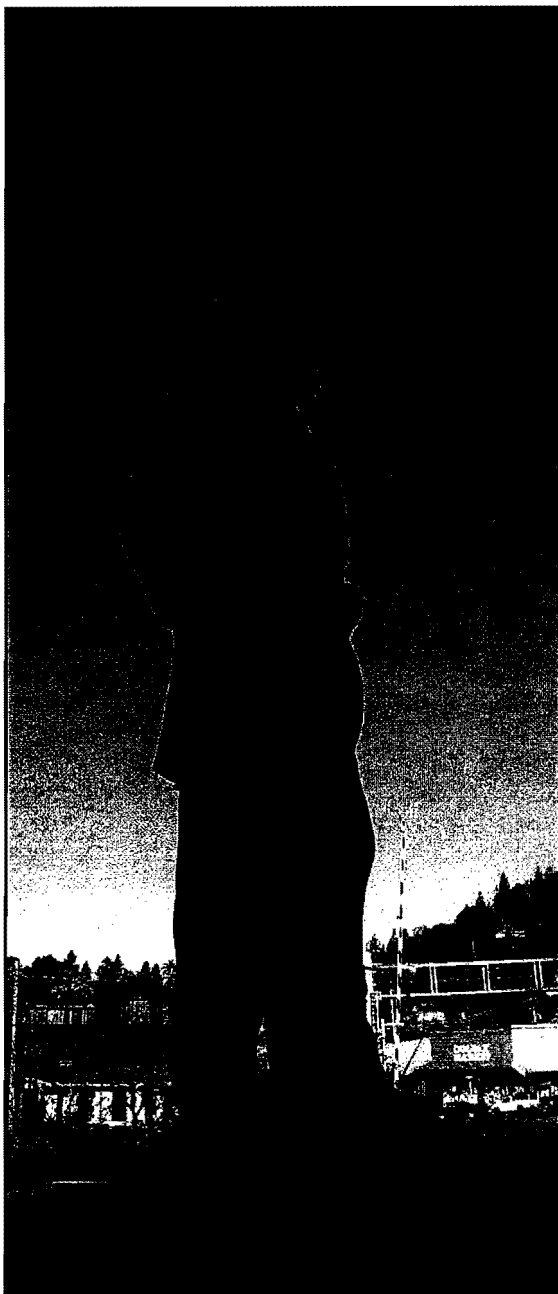
# City of Colfax Historic District Draft Design Guidelines



Prepared for the City of Colfax  
October 2009

Prepared by:

 Foothill Associates

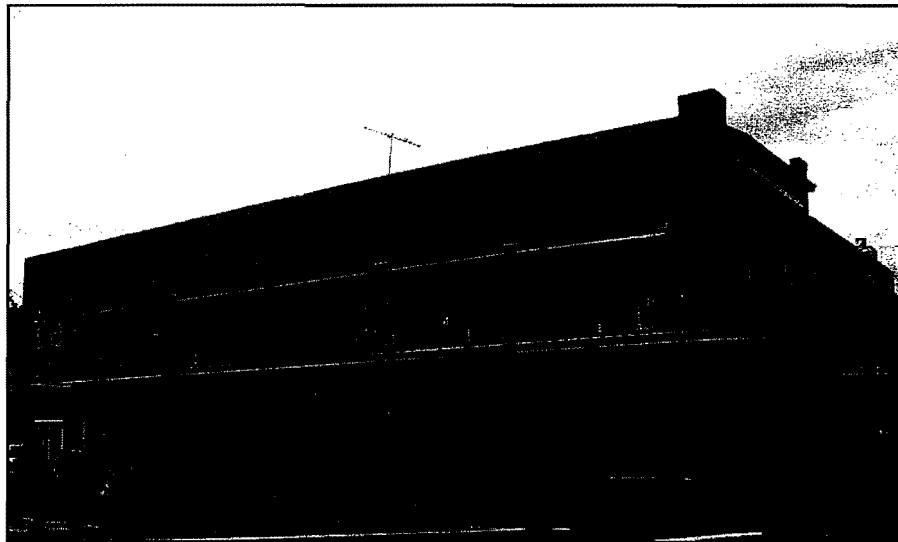


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# 1.1 HISTORIC DISTRICT DESIGN GUIDELINES PURPOSE

The Historic District Design Guidelines implement the Community Design Element in the General Plan by expanding upon the City of Colfax Design Guidelines for properties located within the Historic District of the City.

While the zoning designations within the Historic District allow various commercial, light industrial and residential uses, Single Family Residential properties are exempt from the City of Colfax Design Guidelines. Therefore, the Historic District Design Guidelines only address development and renovations to structures on parcels zoned for commercial, mixed-use, and multi-family residential uses within the Historic District. Any proposed modifications to structures built prior to 1940 should be carefully reviewed against the Historic District Design Guidelines and the City of Colfax Design Guidelines in order to preserve the unique architectural qualities that are the most significant character defining features of the Historic District. Both sets of guidelines should also be consulted for modifications to buildings within the Historic District constructed after this date and for new structures in order to preserve the overall historic aesthetic of the District.



These guidelines apply only to the exterior portions of buildings visible from the street or any public right-of-way. There is no intent in these guidelines to monitor or restrict interior arrangements, design, or décor. Compliance with the Historic District Design Guidelines does not exempt a project from compliance with the additional requirements of the City of Colfax Design Guidelines.

Nothing in these guidelines shall be construed to interfere with maintaining a property according to the City's codes for the health, safety, and welfare of the public.

# 1.2 EXISTING PLANNING GUIDELINES

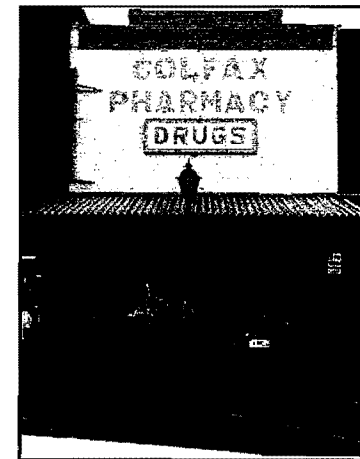
## City of Colfax Municipal Code

Title 17 of the City of Colfax Municipal Code provides the zoning laws that regulate land use and structures in the City of Colfax. Chapter 17.96 establishes the Historic District and City of Colfax Design Guidelines as mechanisms to provide mitigation of the Community Design element and implementation of the Colfax General Plan. Other chapters and sections under Title 17 provide specific direction for each of the City's zoning designations, such as criteria for uses, structure height, lot width, building area, and parking. These Historic District Design Guidelines do not supplant or negate any such requirements of the Municipal Code, but are intended to provide further design direction to ensure that the aesthetic character of the Historic District is preserved.

## City of Colfax Design Guidelines

The City of Colfax Design Guidelines are established under Chapter 19.96 of the Municipal Code. They are intended to preserve and extend the qualities and special characteristics of the Colfax community by encouraging the use of design elements that reflect the existing community style. The following considerations are included in the City of Colfax Design Guidelines:

- Research and/or a visual inventory of the surrounding community building architecture, colors, landscaping and signage is recommended.
- Respect the design of the existing structures as products of the design philosophy and reflection of a specific time.
- Retain and restore the distinctive stylistic features of existing structures.
- Replace or repair lot features (i.e. fencing) when possible.
- Restore historical elements of original building design to create the visual appearance of the original structure.
- Minimize the alterations to historic structures. Façade changes should reflect and conform to the appearance of the surrounding area.
- All new and remodeled structures should reflect the early railroad and/or a mountain/western style of architecture.



- Traditional building materials such as simulated wood shingles, lap siding, board and batten , brick, and indigenous rock should be used.
- Utilize varied materials, textures, or colors to create horizontal and vertical articulation. A range of selected colors is available at City Hall.
- Porches or covered entries are encouraged.
- Residences should be located toward the front line setback, and roofs should be pitched.
- Detached and rear access garages are encouraged.
- Garage doors should not be the focal point of a residence.
- Front doors and porches should be the most prominent aspect of residential design.
- Residential housing or land use conversion to commercial should reflect characteristics and architectural features common during the period of 1875 to the early 1940's. Commercial/Business design characteristics should reflect architectural features common during the period of 1875 to the early 1930's.



The City of Colfax Design Guidelines also provide specific direction on a number of design elements. These include commercial/business buildings, equipment/utilities, lighting, signs, landscaping, parking lots, sidewalk treatments, and landscape maintenance. The Historic District Design Guidelines do not supplant or replace the City of Colfax Design Guidelines. The Historic District Design Guidelines are intended to supplement the City of Colfax Design Guidelines by providing further aesthetic clarification and detail relevant to the particular historic character of the Historic District.

The City of Colfax Design Guidelines apply to all zones within the city limits, except R-1 Single Family Residential zones. Properties are subject to the design review requirements associated with the underlying zoning designation. Properties in the Historic District are further subject to design review for:

1. Modifications to stairways, porches, windows;
2. Paint colors, building materials;
3. Landscaping (when applicable);
4. All signage;
5. Additions or exterior alterations to existing buildings; and
6. All new development.

## Other Historic Preservation Guidelines and Requirements

### ***California Environmental Quality Act***

In addition to the requirements of the Colfax Design Guidelines, buildings with historic significance as defined by the California Environmental Quality Act (CEQA), regardless of zoning, are also subject to historic preservation requirements of the State of California. Compliance with the City of Colfax Design Guidelines does not eliminate these requirements, nor is it a substitution for compliance with CEQA. The definition of "historical resources" is contained in Section 15064.5 of the *CEQA Guidelines* and Section 18955 of the *California Health and Safety Code*. In general, a building is considered to be "historically significant" if it meets the criteria for listing in the California Register of Historical Resources (see Appendix A).

### ***U.S. Secretary of the Interior's Standards for Rehabilitation***

The federal government also provides direction on preservation of historic architectural resources through the U.S. Secretary of the Interior's Standards for Rehabilitation. For federal purposes, a property is considered historic if it is listed in or is formally determined to be eligible for listing in the National Register of Historic Places. Rehabilitation is defined as the act or process of making possible a compatible use for a property through repair, alterations, and additions while preserving those portions or features which convey its historical, cultural, or architectural values. Thus, the federal Standards for Rehabilitation recognize the need to modify structures from their original use to maintain functionality and safety, but direct that such changes should be made in a manner that preserves the historic character of the structure.

Renovations to historic buildings should follow the U.S. Secretary of the Interior's Standards for Rehabilitation, as summarized in Appendix B. Additional technical information on restoration and care of historic buildings is available from the National Park Service's Technical Preservation Services branch, or by visiting their web site at <http://www.nps.gov/history/hps/tps/tax/rhb/stand.htm>.

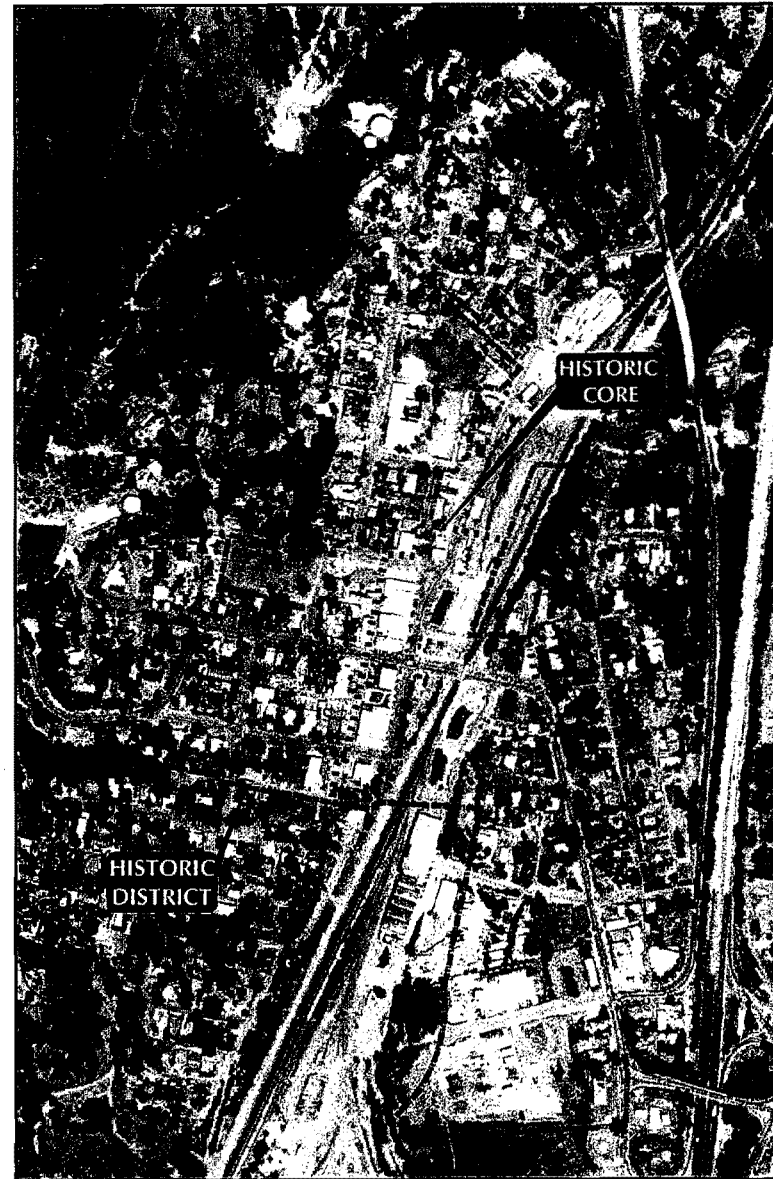
# 1.3 HISTORIC DISTRICT LOCATION

The boundaries of the Historic District of the City of Colfax are illustrated in Figure 1. At its heart is the Historic Core, which generally encompasses the intersection of Main Street and Grass Valley Street, and reaches to just beyond Depot Street on North Main Street to the north and just beyond South Main Street at Church Street to the south. The buildings in the Historic Core include some of the City's oldest commercial structures with many dating from the last half of the 19th Century.

Another prominent feature of the Historic District is the railroad line which bisects the district from north to south. The identity of Colfax as one of the earliest prominent railroad cities in California is well preserved in the historic passenger and freight depot buildings also found within the Historic District.

The Historic District also includes a variety of other commercial/retail, residential, and light industrial uses. The commercial/retail uses are clustered around the Historic Core and along South Auburn Street. Light industrial uses are situated at the far north and south of the district adjacent to the railroad tracks. The remaining areas of the district are zoned primarily for single family residential uses, except for the area between Oak Street and East Church Street to the west of South Auburn Street.

Refer to the City of Colfax zoning map for specific parcel boundaries and zoning designations included in the Historic District. These Design Guidelines apply to the entire Historic District.

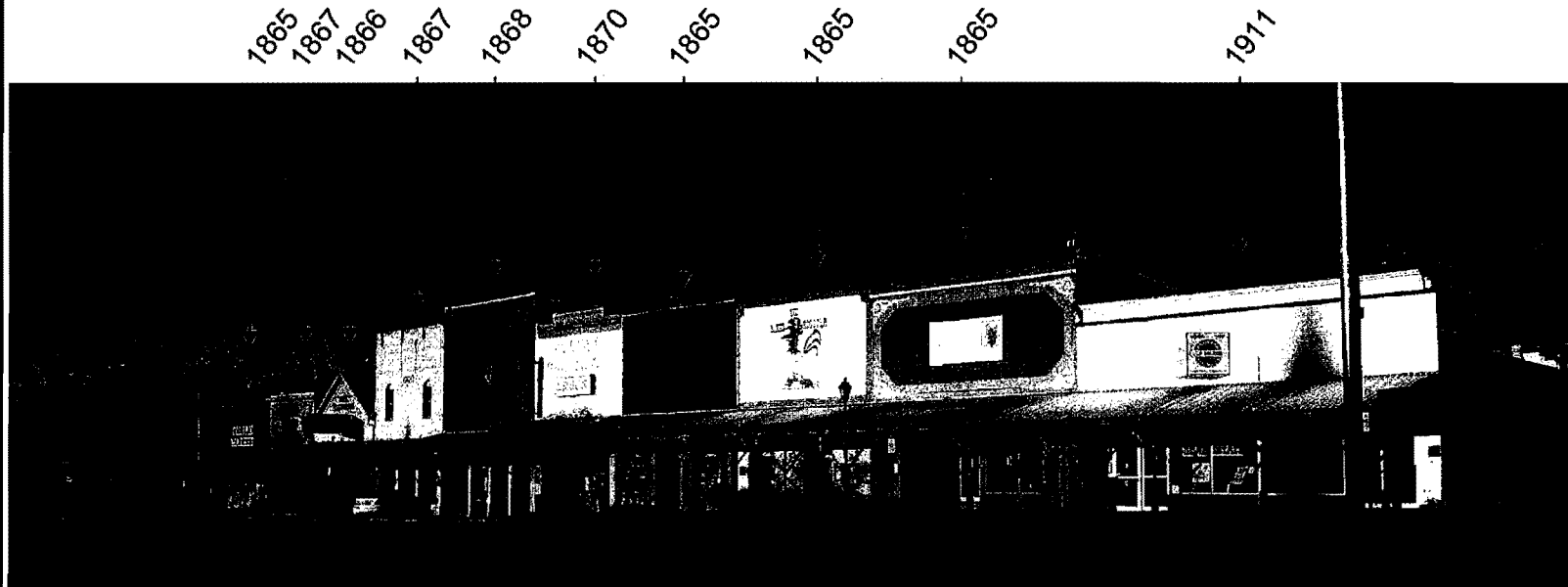


**Figure 1—City of Colfax Historic District**

## 1.4 ARCHITECTURAL STYLES

The architectural character of the Historic District results from a number of factors. The original intended uses of the buildings, available materials, and construction techniques played a major role in the way these structures were designed. Their design was also influenced by the architectural styles that were popular at the time they were constructed or renovated. The result is a blend of architectural elements and features that in itself has a unique character recognizable in many historic town centers throughout the Sierra foothill communities.

While no single architectural style defines the buildings in the Colfax Historic District, elements from several predominant styles recur frequently. The City of Colfax Design Guidelines refers to these generally as the early railroad and mountain/western styles. This information about the Historic District architectural styles is intended to further refine the understanding of specific architectural styles represented in the buildings constructed before and including the early 1930's. New construction and renovations to historical buildings should seek to replicate these typical materials, features, finishes, motifs, and proportions as feasible to preserve the architectural aesthetic integrity of the Colfax Historic District.



*These buildings in the Colfax Historic Core illustrate the Gold Rush/Early Western style of commercial structures.*

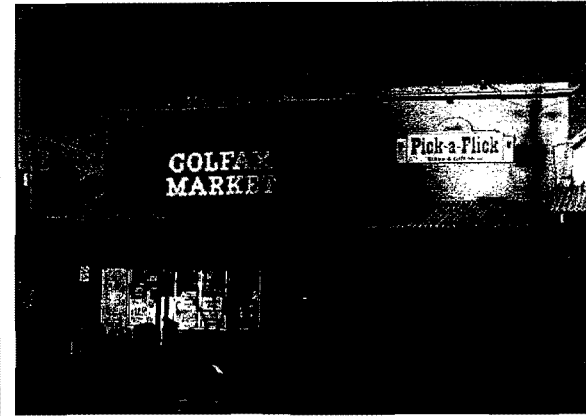
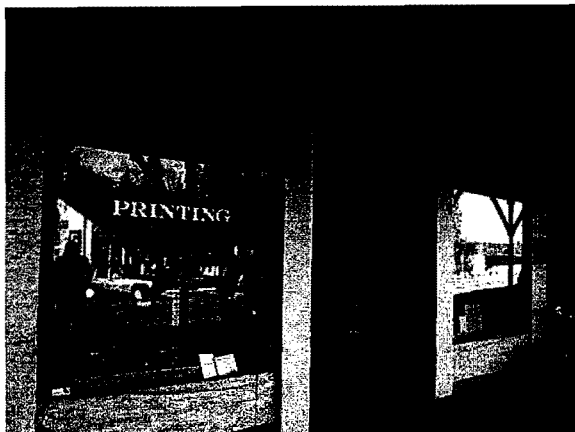
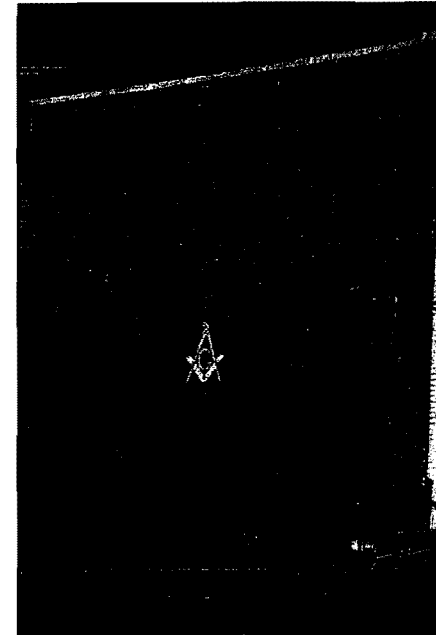
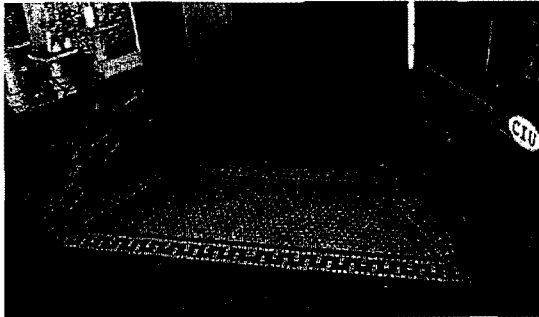


## COMMERCIAL BUILDING STYLES

### Gold Rush/Early Western Style (circa 1860-1915)

Most of the commercial buildings in the Historic Core were built during this period. Even though subsequent renovations have modified the appearance of some buildings, many of the defining characteristics are still evident. These include:

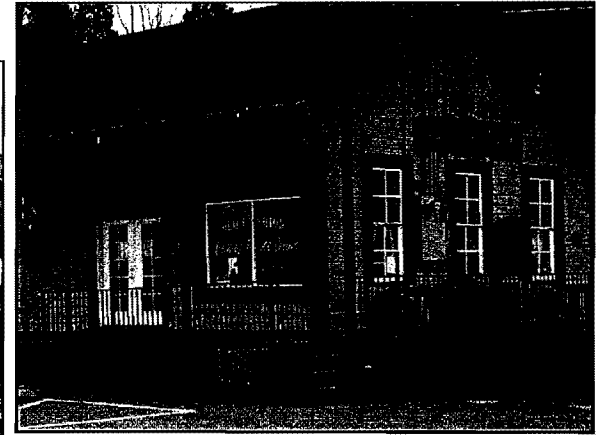
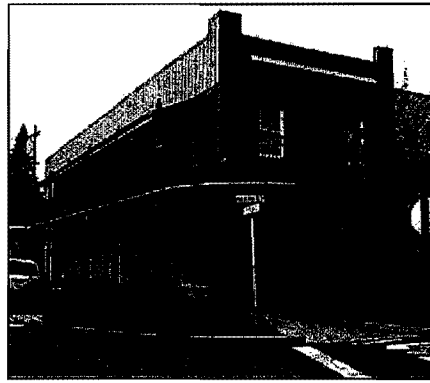
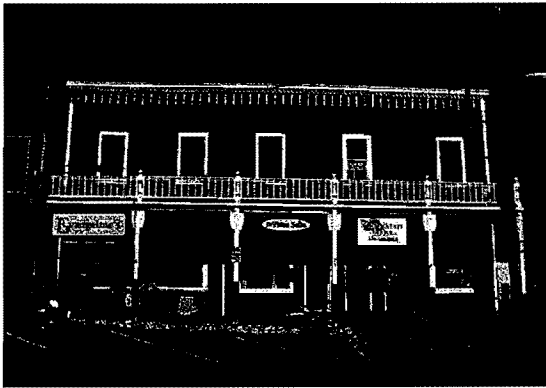
- One to one-and-one-half stories in height
- Gable roof or flat roof with false front
- Brick or veneer walls
- Decorative parapet and cornice
- Recessed entries with decorative tile work
- Portico with shed awnings on wooden posts
- Period door and window hardware



### Eclectic Style (circa 1880-1920)

The Eclectic style is used to describe buildings that combined elements from other popular styles of the time. This combination of elements is the defining characteristic of this style. Eclectic style buildings often:

- Include adaptations of Arts and Crafts (Stickley), Victorian (Eastlake), and sometimes Early Western features
- Combine materials such as wood and stucco on the exterior
- Have decorative detailing eaves, windows, railings, etc.



### Mission Revival Style (Circa 1910-1930)

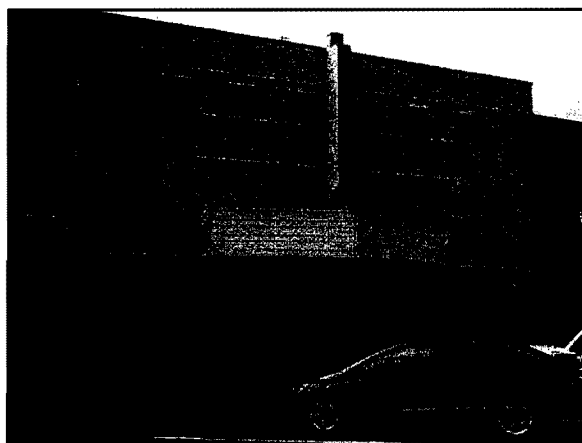
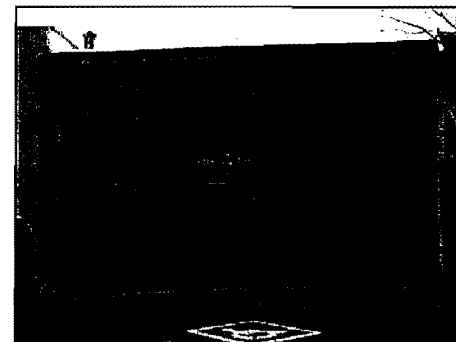
The Mission Revival style reflects the popular romantic interest in the early days of the California missions. Typical features include:

- Stepped arches on the parapet
- Gabled or hip roof with red clay barrel tiles
- Stucco walls
- Arched windows, doors, or accents
- Structural accents such as a pergola, trellis, loggia, or arcade
- Tile, wood, or wrought iron details

## Early 20th Century Commercial (circa 1900-1935)

Many commercial buildings constructed in California during this period reflect classical influences derived from the Italian Renaissance, thus this period is also sometimes called the Second Renaissance Revival. Examples of this style in the Colfax Historic District are distinguished more by their proportions than by ornamentation. Defining characteristics include:

- Brick or stucco construction
- Symmetrical proportions often with a strong visual division between elements
- Regularly spaced, rectangular window openings
- Larger windows on the ground level, smaller windows above
- Cornice with understated decoration



## Art Deco (circa 1920-1940)

The Art Deco style of architecture emerged in American commercial buildings after World War I. It was part of an overall interest in all things modern that characterized many aspects of popular culture including art, music and literature. Defining characteristics include:

- Emphasis on clean, stylized forms
- Symmetry and geometric shapes
- "V"-shaped forms suggesting speed and aerodynamics
- Ornamentation as detailing to emphasize form

Many modern and renovated commercial structures in the Historic District help preserve the character of the district by incorporating design features and materials from the historic architectural styles found throughout the district.



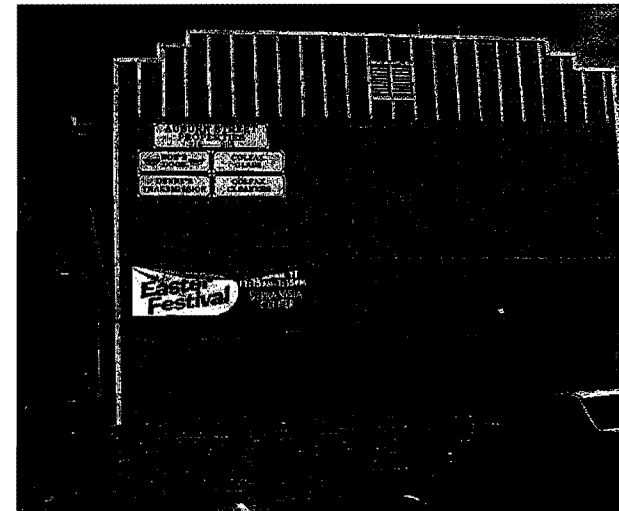
*The gazebo in Roy Tom's Plaza uses materials, forms, and proportions that are consistent with the Eclectic style of architecture.*



*This commercial building includes a decorative parapet and cornice, color scheme, and pergola with decorative posts and railings that connect it to the Early Western style of architecture.*

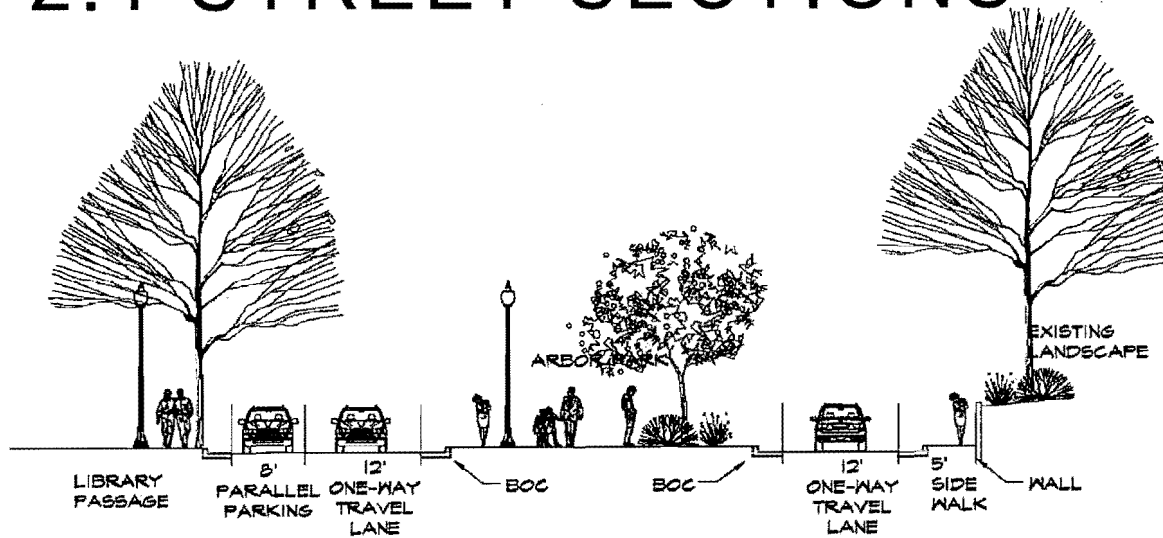


*This modern (1972) structure uses historic design elements such as stepped parapet walls, portico with wooden columns, wooden siding with brick panels, and second story balcony.*

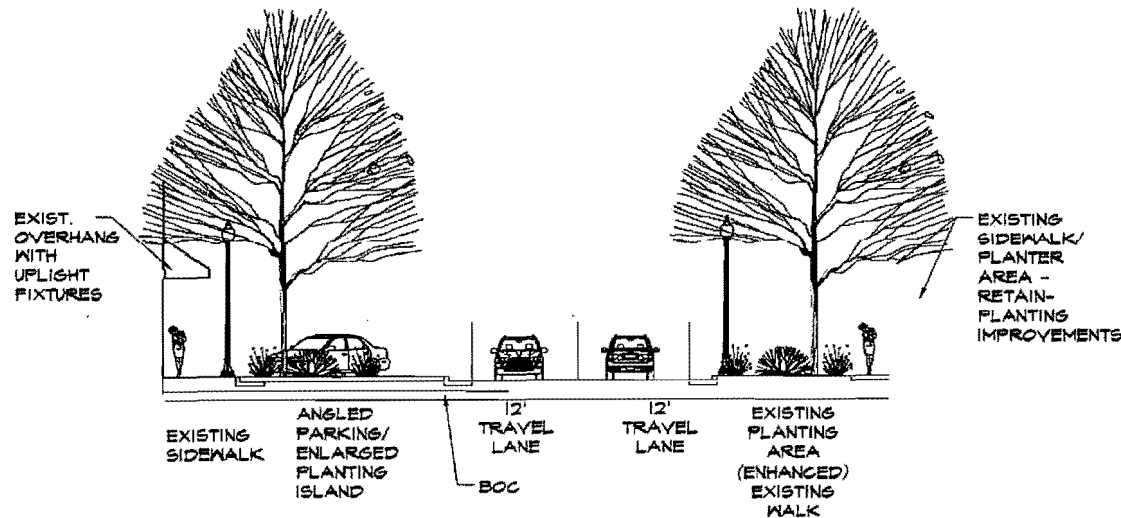


*The board and batten siding with horizontal detailing, stepped parapet, and brick foundation help this building exterior blend in with the character of the Historic District.*

## 2.1 STREET SECTIONS



**Figure 2—South Main Street at the Library**



**Figure 3— North Main Street at the Caboose/ Farmer's Market**

2.1.1 Streets in the Historic District should provide safe and welcoming facilities for vehicular traffic, pedestrians, and bicyclists.

2.1.2 Where feasible, medians with planters should be included in roads with more than 2 lanes to visually break up the expanse of paving and reinforce the pedestrian scale.

2.1.3 Streets in the Historic District should include a separation between pedestrian and vehicular spaces. Buffering can take the form of planting in combination with landscape elements,

2.1.4 Angled parking and planting islands should be used where possible to help slow traffic and buffer sidewalks from the street. Meeting and resting areas, such as plazas with seating, should be included to encourage people to stay longer in the Historic District,

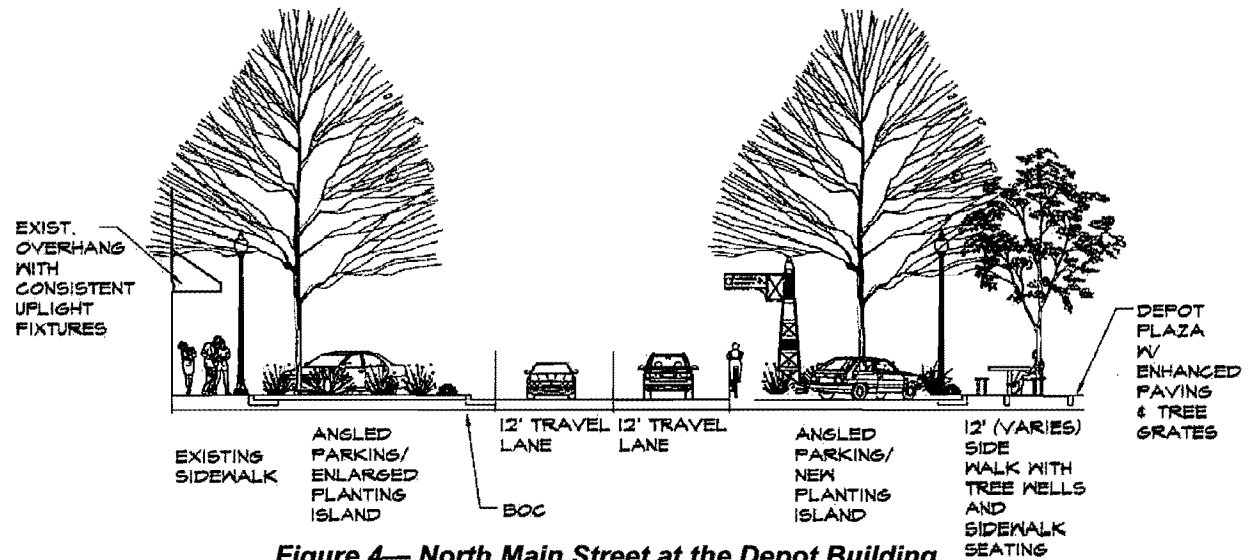


Figure 4— North Main Street at the Depot Building

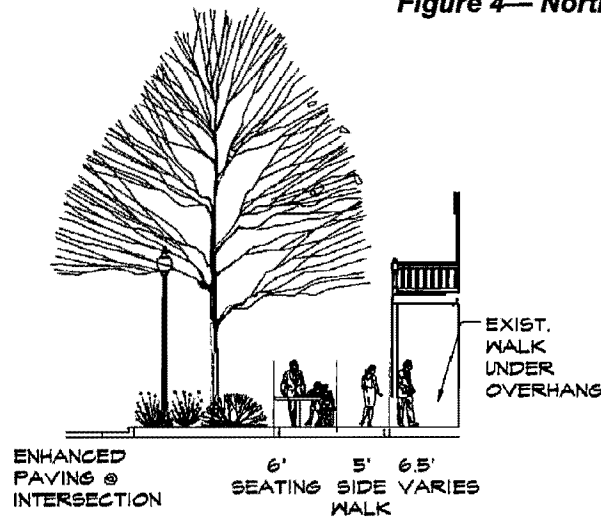
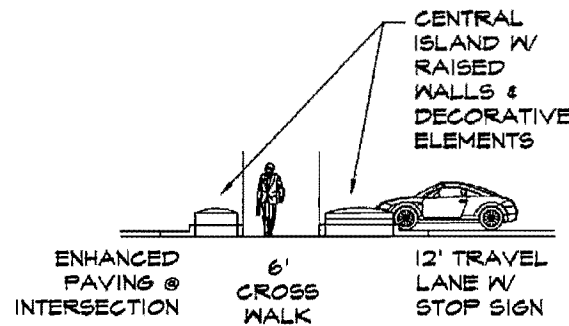


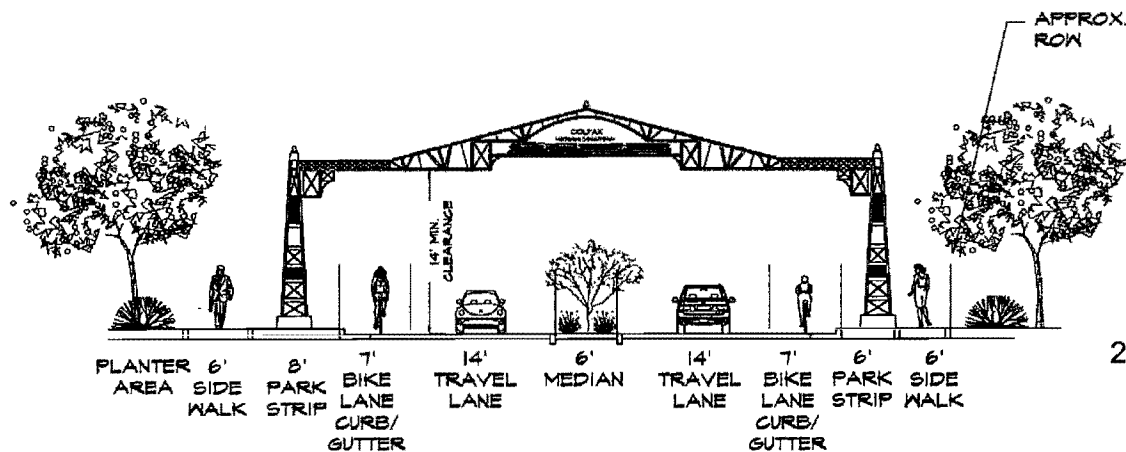
Figure 5— North Main Street at Depot Street

2.1.5 Integrate small seating areas, benches, gathering spaces and alcoves at intervals adjacent to the sidewalk outside of businesses to facilitate pauses and interaction among visitors. Sidewalk may be widened to accommodate such uses where widening does not interfere with overall street circulation and traffic flow.



**Figure 6 North Main Street — Pedestrian Crossing**

- 2.1.6 Used decorative paving treatments for crosswalks such as clay or concrete brick, or stamped concrete to make crossings more visible to motorists and pedestrians. Raised curbing with decorative panels and inset ornamental elements may also be used to further enhance pedestrian safety



**Figure 7 - Grass Valley Street as Main Gateway to Historic Downtown**

- 2.1.7 Erect gateway features at major entry points and to direct visitors to the Historic downtown. Possible locations include the Highway 174 access to North Main Street, Grass Valley Street, South Auburn Street, and South Canyon Way. Gateways should reflect the historic quality and heritage of the City.

- 2.1.8 Include bike lanes throughout the historic downtown to improve the safety of bicycle users, and narrow vehicular lanes to slow traffic speed.

## 2.2 ROUNDABOUTS AND PEDESTRIAN CROSSINGS

- 2.2.1 Crosswalks should be delineated with a change in the paving material, pattern, or color.
- 2.2.2 Mid-block crosswalks should be highlighted to increase driver-awareness using in-pavement lighting, a raised walkway, painted limited lines, or other approved methods.
- 2.2.3 Simulated brick inlay is the preferred style for crosswalks in the Historic Core.



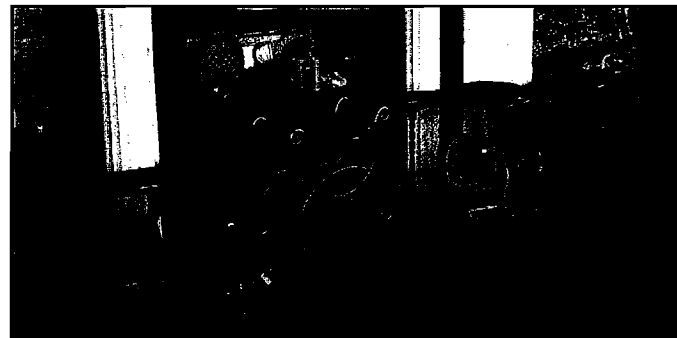
- 2.2.4 Roundabouts should be considered as a traffic control measure.
- 2.2.5 Roundabouts may also serve as a location for public art projects.



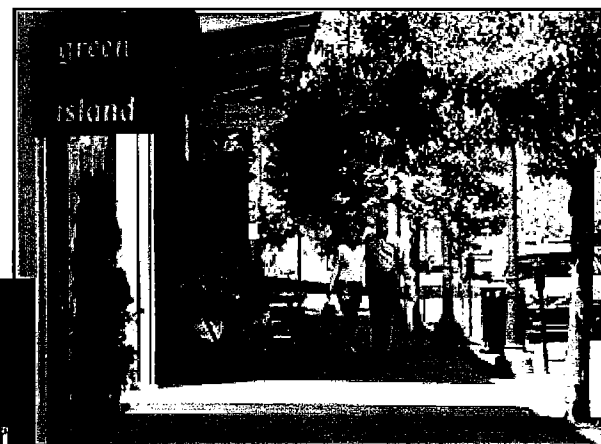
## 2.3 WALKWAYS AND ACCESSIBILITY

The Americans with Disabilities Act (ADA) mandates that places of public accommodation be accessible to all users. The California Building Code (CBC) further regulates accessibility. The guidelines introduced in this document should not prevent or inhibit compliance with accessibility laws. Consult with the State Historic Preservation Office for more information regarding compliance and alternative design options for meeting accessibility requirements in historic buildings.

- 2.3.1 Pedestrian access should be maintained between all parking areas, buildings, transit stops or other major points of interest.
- 2.3.2 Primary pedestrian routes are the most direct connections between locations. Sidewalks are typically the primary pedestrian route.
- 2.3.3 Primary pedestrian routes shall be a minimum of 4-feet in width and composed of a universally accessible, all-weather material, such as concrete paving, asphalt, or modular pavers.
- 2.3.4 Secondary pedestrian routes are routes that indirectly connect two points of interest that are served by primary routes. These may include side paths in parks or open spaces.
- 2.3.5 Secondary pedestrian routes may be surfaced with decomposed granite in addition to the materials acceptable for a primary route.
- 2.3.6 Ramps shall be constructed as needed along all pedestrian routes to meet the standards of the Americans with Disabilities Act and California Building code, except as exempted due to historic preservation standards.



*Storefront displays should be arranged to maintain a pedestrian walkway of at least four-feet in width.*

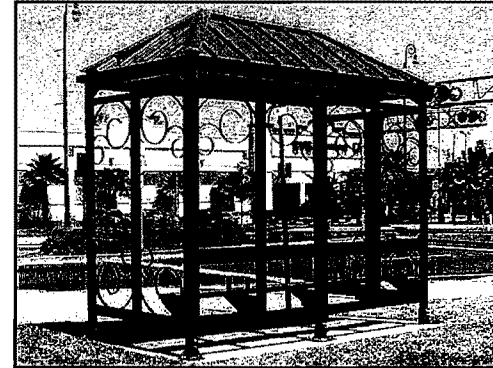


*Pedestrian routes should be wide enough to accommodate street furniture and planting, while allowing easy passing by small groups of people.*

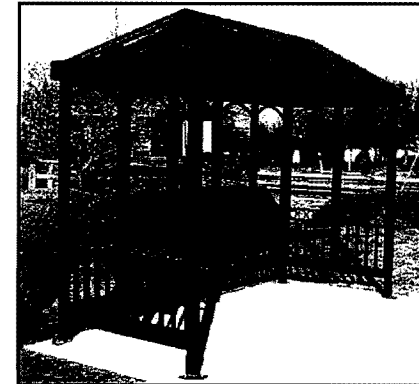


## 2.4 TRANSIT AND CONNECTIVITY

- 2.4.1 Public transit access should be integrated into the Historic District circulation plans.
- 2.4.2 Transit shelters should be placed on primary pedestrian routes.
- 2.4.3 Transit shelters should be constructed of metal, brick, or wood in a style similar to the surrounding buildings,.
- 2.4.4 The train station should be linked to the commercial center of the Historic Downtown via a primary pedestrian route to encourage train passengers to visit nearby businesses.



*Transit shelter styles should complement the historic architecture of the surrounding area.*



*A clear, safe pedestrian route from the train station to the Historic Core is needed to encourage visitation and shopping.*

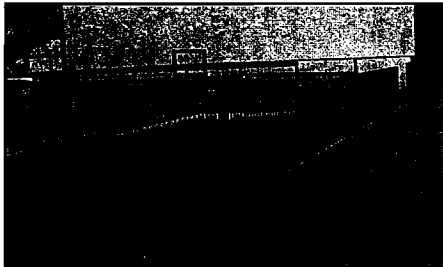


# 3.1 FENCING, WALLS, SCREENING, AND ENCLOSURES

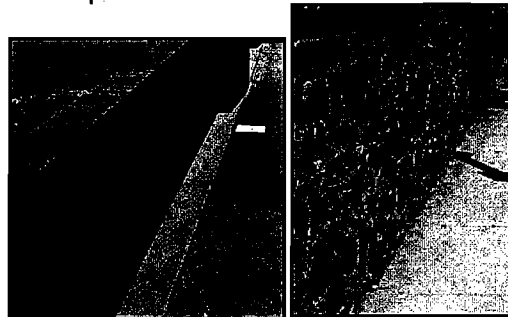
- 3.1.1 Metal fencing should be black or painted or powder coated in colors appropriate to the building.
- 3.1.2 Ornamentation appropriate to the historic building such as arrow finials or rings may be used on fencing.
- 3.1.3 Fences and railings should be “transparent” in nature and allow through views unless they are used to screen non-historic features, such as dumpsters.



*Metal railing with arches and spearhead finials, painted to match the building.*



*A simple metal fence.*



*Brick or stone walls should be consistent with the adjoining buildings.*



*Split rail fences are appropriate for open space areas.*

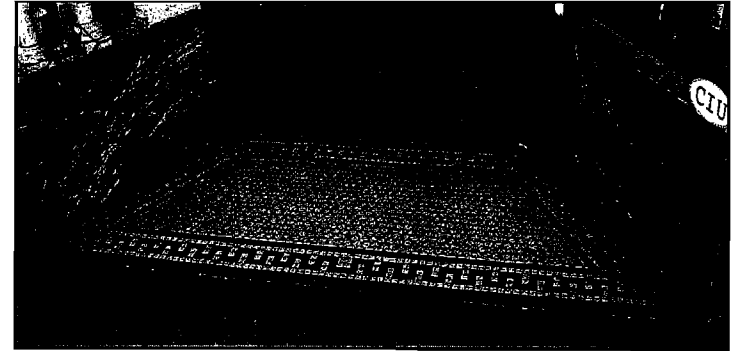


*Chain link or plastic fencing are not appropriate materials.*

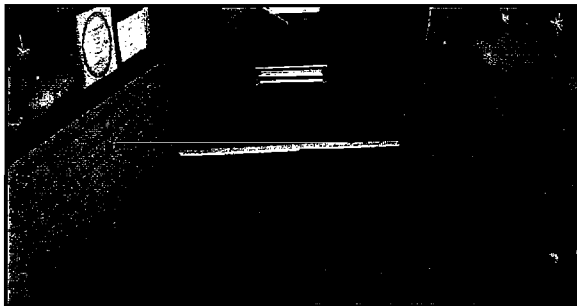
- 3.1.4 Brick may be used for walls adjacent to brick masonry buildings. Brick sizes and pointing should match adjacent buildings.
- 3.1.5 Split rail wood fencing may be used in parks and open space areas.
- 3.1.6 Chain link, plastic, plywood, fiberglass, and concrete masonry unit (CMU) walls are not appropriate.

## 3.2 BUILDING OPENINGS & FACADES

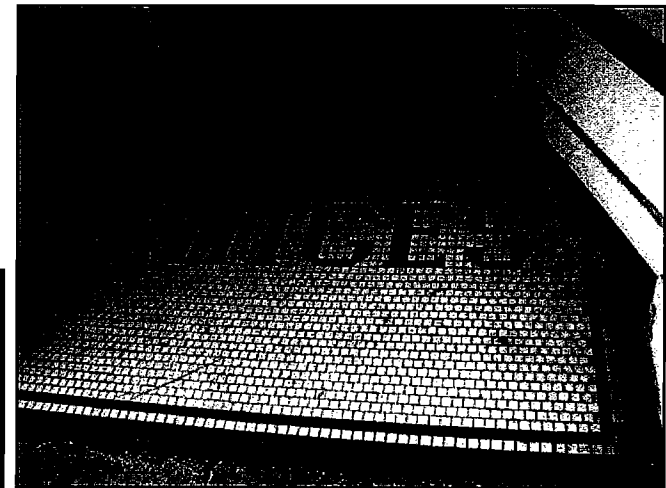
- 3.2.1 Avoid creating additional openings or removing existing ones on facades visible from the street or public right-of-way.
- 3.2.2 Avoid the removal of historic windows and frames.
- 3.2.3 If door or window replacement is necessary, use a similar design to match the original style and massing.
- 3.2.4 Maintain original window and door proportions.
- 3.2.5 Use of original materials, such as wood framing, is encouraged. Plastic and aluminum framing is discouraged.
- 3.2.6 Highlight recessed entrances with decorative paving, tiling, or woodwork.
- 3.2.7 Maintain recessed door and window style to reflect the character of the building's architectural style.
- 3.2.8 Do not cover existing windows or doors with solid material, such as masonry, siding, or plywood, except as temporarily necessary for repair or replacement of windows.



*Tiled or mosaic entryways add character and interest to the entrance.*

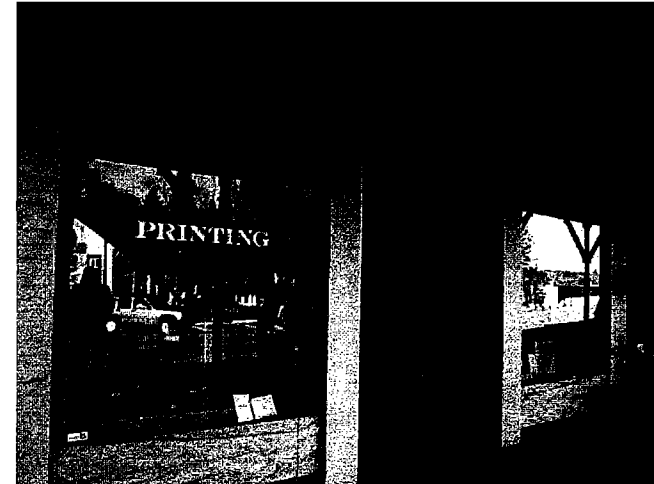


*Recessed doorways are typical of many buildings in the Historic District.*





*Typical recessed opening with large ground floor display windows.*



*Typical building façade elements and proportions for late 19th-early 20th century small commercial buildings.*

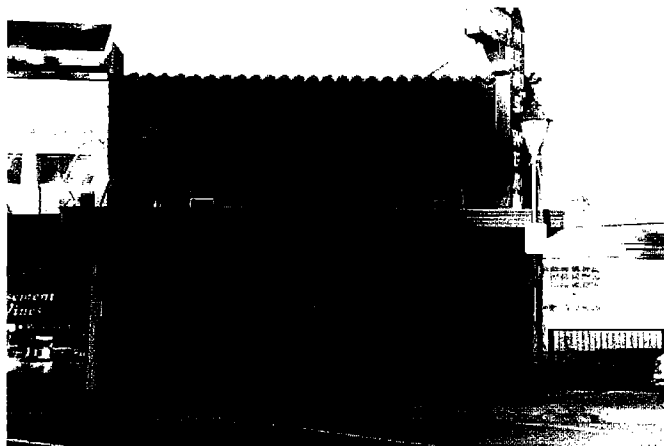
- 3.2.9 Larger ground floor display windows should be maintained.
- 3.2.10 Glass in windows and doors should be clear, unless historic evidence suggests that stained glass was used in upper portions of the doors and windows.
- 3.2.11 Store front elements should be preserved. Typical store front elements to be preserved upper story windows, parapet, signage area above the display windows, transom windows, recessed entryway and kick plate under the display windows, as well as existing woodwork and framing. to be preserved as appropriate.

## 3.3 AWNINGS AND CANOPIES

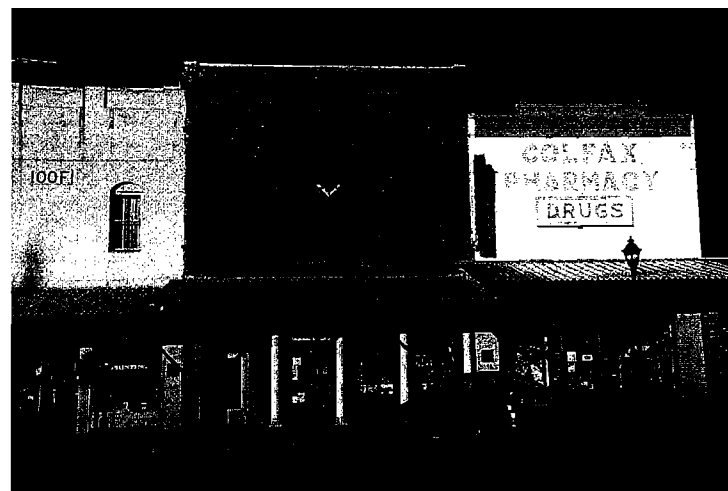
- 3.3.1 Wood, metal, or fabric awnings and canopies are acceptable. Materials and proportions should be architecturally appropriate.
- 3.3.2 Awnings and canopies should be preserved, as well as porch-type structures. If altered, awnings, canopies, and porches should be restored or reconstructed to the original design as much as possible.
- 3.3.3 Awnings and canopies should typically cover the entire width of the building.
- 3.3.4 Continuous awnings along multiple building frontages can vary in height if transition between the canopies fit the architectural mass and do not allow nuisances in crevices between different canopies.



*Balconies create a covered area that functions much like an awning.*



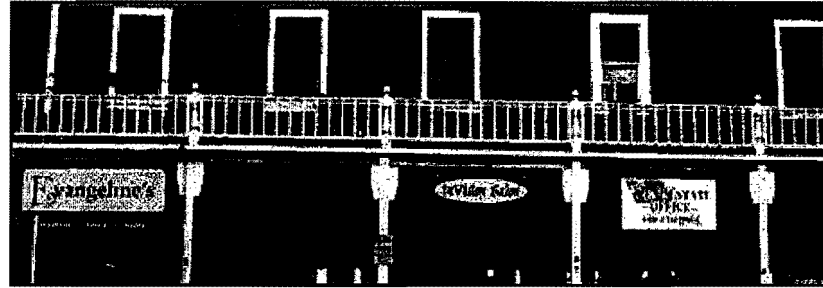
*Awnings and canopies should span the width of building façade.*



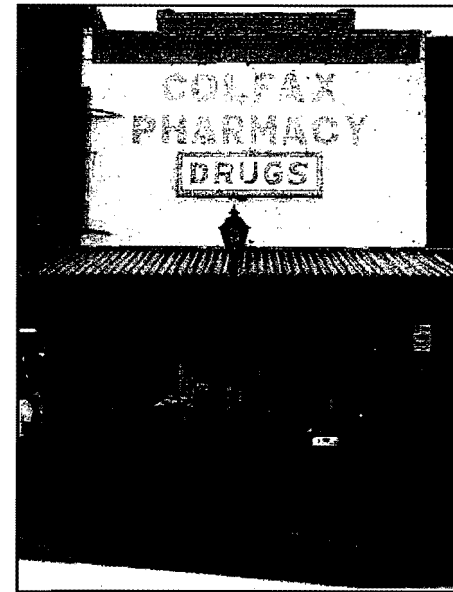
*Continuous awning steps down street grade at breaks in building frontages to fit with massing.*

## 3.4 SIGNAGE

- 3.4.1 Signs consistent with the historic character and proportions of the buildings are encouraged. Signs should not overpower the building or obscure any significant architectural features.
- 3.4.2 Permanent signs should be for the purpose of identifying businesses or historic building names only. No permanent signs displaying advertising or promotions should be permitted.
- 3.4.3 The number, forma, and size of signs must comply with the regulation of the City of Colfax sign ordinance.
- 3.4.4 Signs should not exceed an area more than 2/3 the width of the storefront.
- 3.4.5 New buildings with multiple businesses should have a coordinated sign program for the entire building.
- 3.4.6 Signs utilizing day-glo colors, back or interior lighting, or made of plastic are not allowed in the Historic District. Signs with neon lights or movable lettering should not be allowed, unless historically appropriate for the building as documented in historic photographs or accounts.
- 3.4.7 Sandwich boards, if allowed, should be limited to one per business and should only be displayed during hours the business is open. These signs should be placed so as not to interfere with pedestrian or vehicular access. Temporary signs shall be constructed of materials and colors appropriate to the Historic District.
- 3.4.8 Hanging signs are encouraged and should maintain a minimum clearance of seven (7) feet above grade as feasible.
- 3.4.9 Preservation or restoration of historic wall paintings is encouraged.
- 3.4.10 Window and door lettering should not cover more than 25% of the glass area. Recommended typefaces are described in the City of Colfax Design Guidelines.



*Wooden signs consistent with the character of the building.*



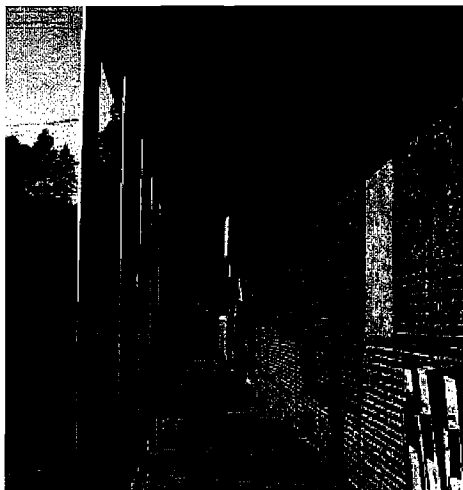
*Historic wall paintings and signs should be preserved.*

## 3.5 EQUIPMENT AND SERVICES

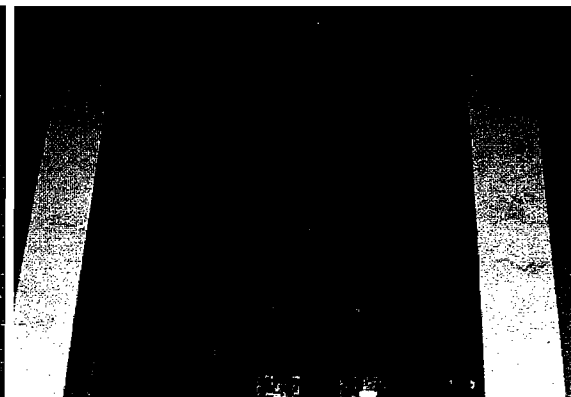
- 3.5.1 Utility installation, undergrounding, and screening shall comply with the requirements in the Colfax Municipal Code to ensure public safety.
- 3.5.2 Equipment and services such as electrical transformers, propane tanks, air conditioners, telephone lines, electrical lines, ventilation systems, meters, and telecommunication equipment should be located to minimize visual impact and maintain architectural integrity of historic structures.
- 3.5.3 Screening of equipment and services may be accomplished with a variety of means, such as planted buffers or fences constructed to complement the architectural style of nearby buildings.



*Utility pole and multiple cables obstruct the view of this historic structure.*

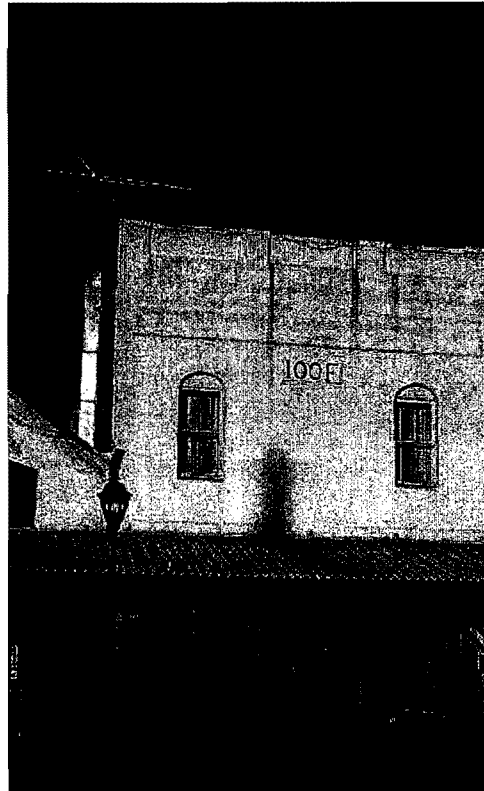


*Incorporating modern equipment upgrades in historic buildings can be challenging.*





*Locate equipment and services where they will not detract from the historic character of buildings.*



*Above ground utility wires can conflict with trees, creating maintenance and safety issues.*



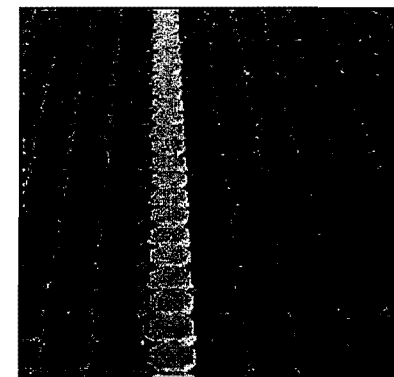
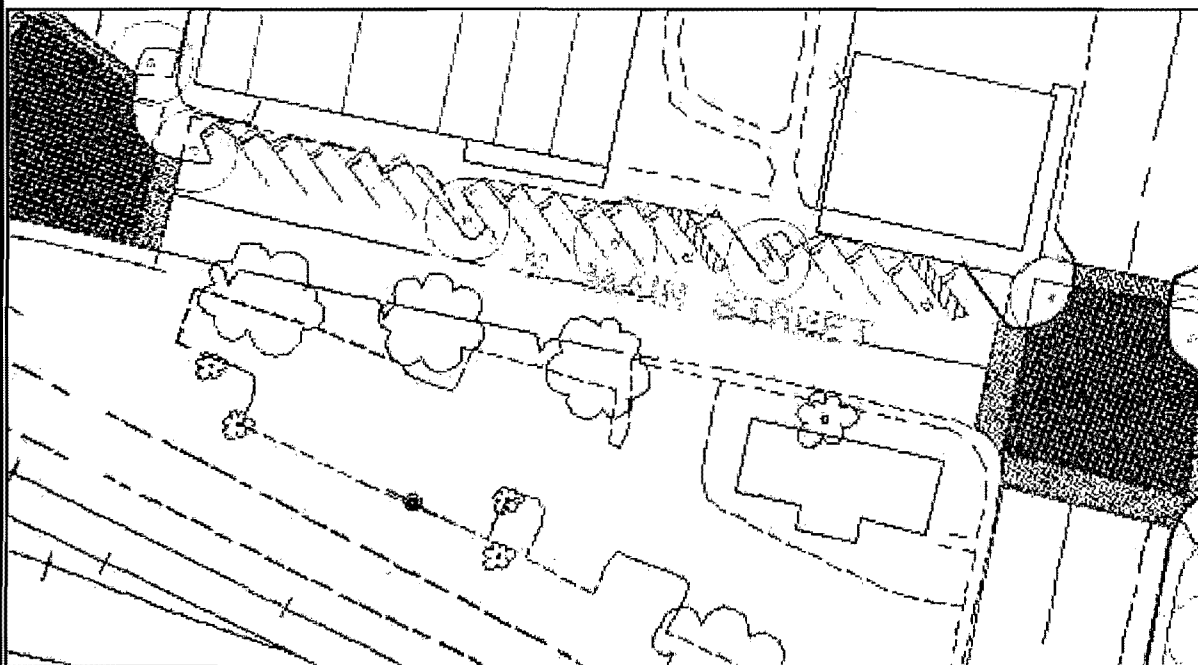
- 3.5.4 Screening for equipment and services may be integrated in the building envelope if screening structure does not detract from historical appearance. Match building material and color.
- 3.5.5 Where feasible, consider moving overhead utility wires underground. Overhead wires can detract from historic architecture, constrain tree planting, and impact scenic views.
- 3.5.6 Dumpsters should be located to the rear of buildings out of public view yet still accessible to service vehicles.

## 3.6 PARKING AREAS

- 3.6.1 On-street parking should be placed diagonally and have a bulb-out planter every 7-9 spaces to act as a traffic calming measure while softening the appearance of the hard asphalt and providing additional space for shade trees.
- 3.6.2 Parking lot spaces can be delineated with pavers or decorative asphalt treatment to complement intersection paving while also improving street paving aesthetics.



*Changes in paving material can be used to delineate parking spaces.*

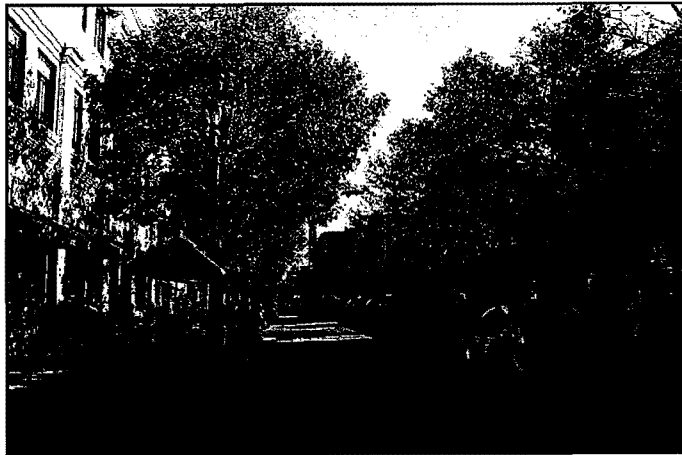


## 4.1 LANDSCAPE CORRIDORS, MEDIANS AND FEATURES

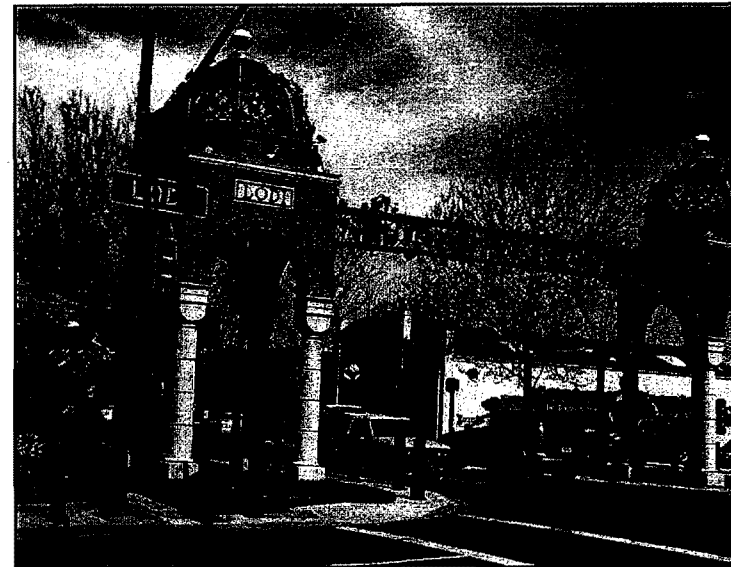
- 4.1.1 Landscape features and medians should enhance the overall aesthetic values, while providing a space for shade trees, art installations, and signage.
- 4.1.2 Landscape corridors and medians should be designed to increase pedestrian circulation and slow traffic movement through the area.
- 4.1.3 Include built features such as gateways, directional sign, and interpretive elements at key locations to enhance the unique sense of place and community within the Historic District



*Medians need to be sized in proportion to the street width and available right-of-way.*



*A typical historic downtown district with narrow streets, on-street parking, street trees, and sidewalk cafes promotes pedestrian use.*



*Entry structures can help define the boundaries of the Historic District.*

## 4.2 STREET TREES AND SHADE STANDARDS

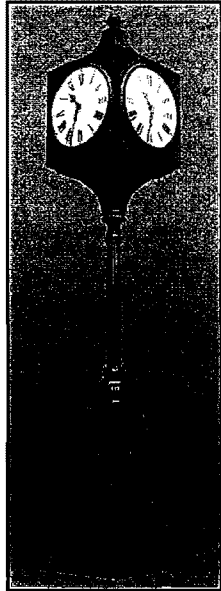
- 4.2.1 All street trees should be in conformance with the City's Street Tree Standards.
- 4.2.2 Plant materials should be adapted to the Colfax climate.
- 4.2.3 Plant materials should be selected for their aesthetic and structural characteristics, such as texture, color, bloom time, and growth habit, to provide year-round visual interest.
- 4.2.4 Street plantings should include a balance of both evergreen and deciduous plants.
- 4.2.5 Dead parking lot trees should be replaced within 3 months of their death with a tree that will mature to an equivalent size.
- 4.2.6 Parking lot shade trees at maturity should have sufficient height, clearance, and canopy to provide shade over vehicles and pedestrians that will pass under or be parked under the trees.
- 4.2.7 Lone street trees in sidewalks or plazas should be placed in planting pits with tree grates to protect against soil compaction of the root zone.
- 4.2.8 Tree grates should match existing architecture and site furniture. Only one style of grate should be used per block.
- 4.2.9 The minimum size for a tree planting pit is 5'x5' or 25 square feet.



*Tree grates protect tree roots and add visual interest to the ground plane.*



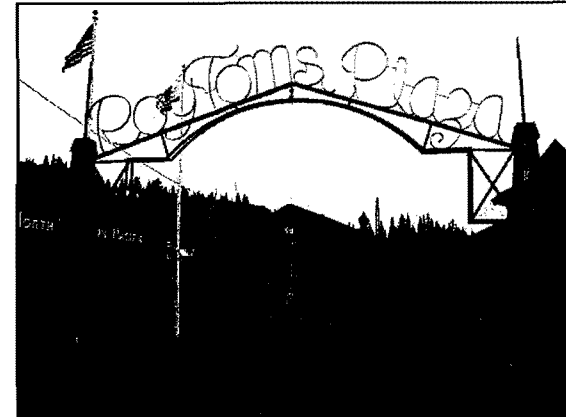
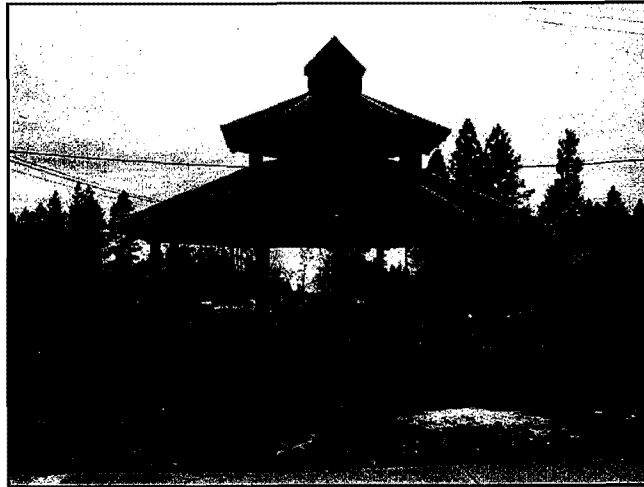
## 4.3 POCKET PARKS AND OPEN SPACE



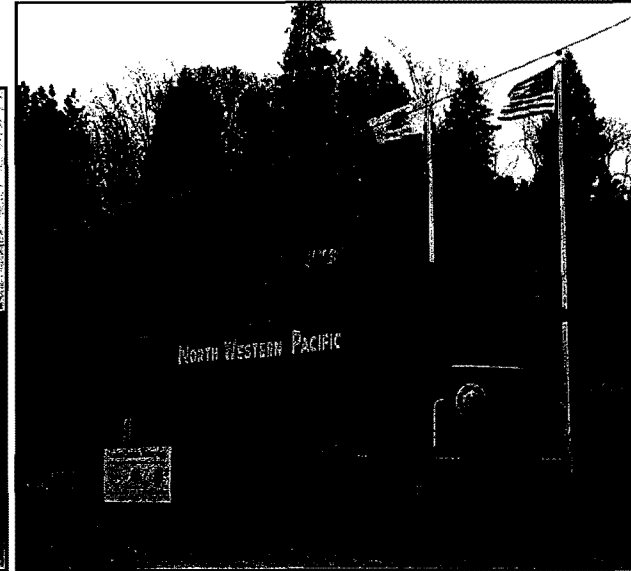
4.3.1 Special attention should be given to areas where the community can gather in large or small groups. These areas should focus on creating both intimate and large gathering areas with public amenities such as tables, benches, drinking fountains, shade shelters, play areas and other uses as specified by the local community.

4.3.2 Furnishings and art pieces for these gathering areas should reflect the local community style with an emphasis on The Gold Rush and Victorian Eras.

*A clock or other vertical sculptural element can serve as the focal point of a gathering area.*

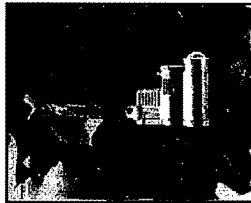


*The gazebo, caboose, and entry arch are all defining features of the open space in the Historic District.*

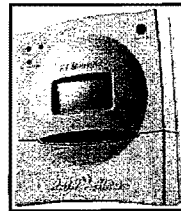


## 4.4 IRRIGATION

- 4.4.1 All planted areas in the public right of ways must be irrigated with an automatic control system with a weather monitoring option to only allow watering when it is needed.
- 4.4.2 Irrigation valves shall be placed in subsurface plastic valve boxes within the planting areas.
- 4.4.3 Above-ground irrigation equipment, such as a controller and backflow prevention device shall be installed in metal enclosures with locking access door.
- 4.4.5 Weather, rain, and moisture sensors should be utilized to create the most water-efficient irrigation system possible.
- 4.4.6 Drip, subsurface, or bubbler irrigation systems should be used in all planting areas less than 5' wide.



*All irrigation should utilize smart controllers that integrate weather sensing technology and automatically adjust sprinkler run time.*

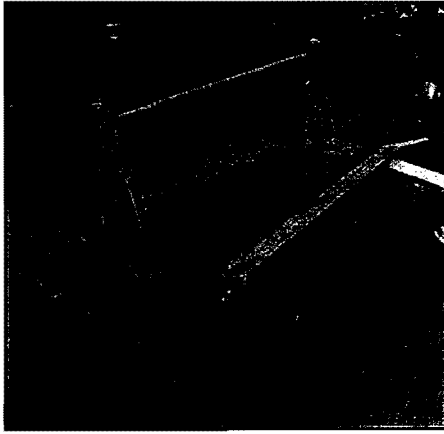


*All valves shall be placed in an in-ground valve box.*



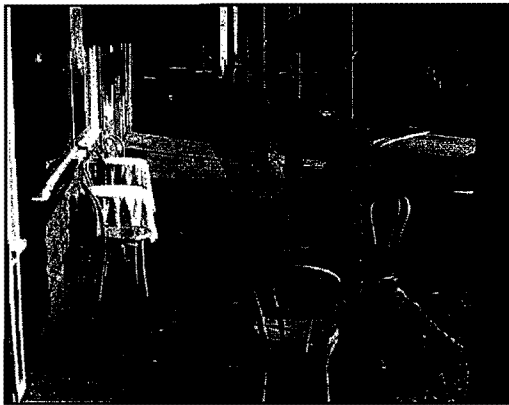
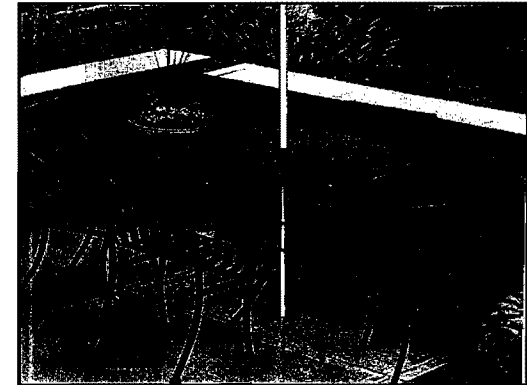
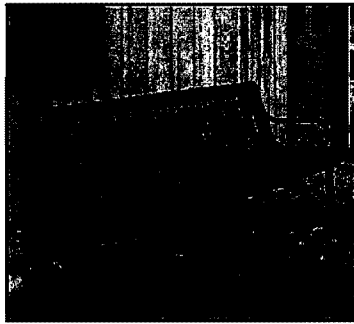
*Meters and other above-ground equipment shall be protected from vandalism by sturdy but visually unobtrusive enclosure.*

## 4.5 BENCHES AND TABLES



### Benches

- 4.5.1 Benches placed in front of buildings shall not block access to the building or interfere with the accessible route of travel outside the building.
- 4.5.2 Benches should match the historic architectural style of their surroundings.
- 4.5.3 Bench material should be primarily wrought iron or wood in appearance.
- 4.5.4 Benches should be black, brown, bronze, or verdigris in color.
- 4.5.5 Benches should be designed for accessibility.
- 4.5.6 Benches should be located at major pedestrian nodes and in public open spaces.



### Tables

- 4.5.7 Tables and chairs placed outside a business should be matched in sets or stylistically consistent.
- 4.5.8 Tables should be constructed of wood or metal and be smoothly finished.
- 4.5.9 Movable tables and chairs should be stacked against the building or stored inside when the business is not open.
- 4.5.10 All outdoor site furniture must be sized to maintain a clear accessible route of travel into and past the business.
- 4.5.11 Tables in parks, plazas or other open spaces should be constructed of metal and colored black, brown, bronze, or verdigris.

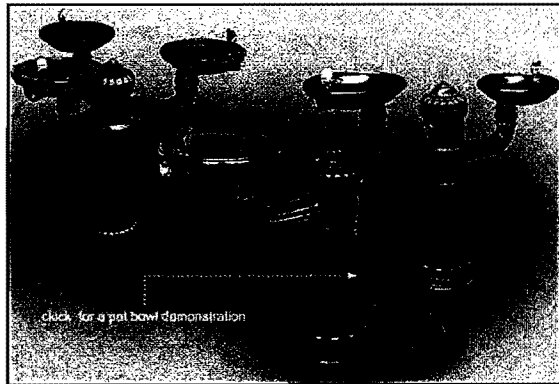
## 4.6 SITE FURNISHINGS

4.6.1 All site furnishings in public spaces should reflect a consistent design that is harmonious with the historic character of the District.

### Trash Receptacles

4.6.2 Trash and recycling receptacles should be square or circular, metal enclosures with side-opening doors for maintenance.

4.6.3 Color shall be black, brown, bronze, or verdigris.



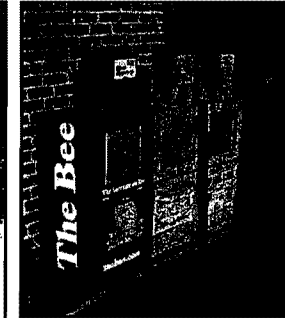
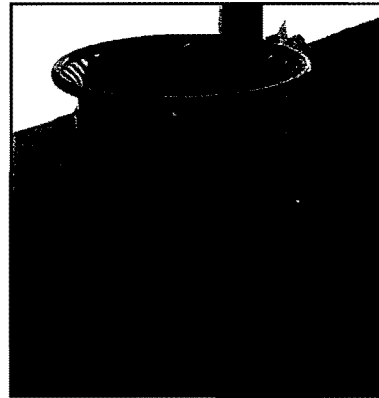
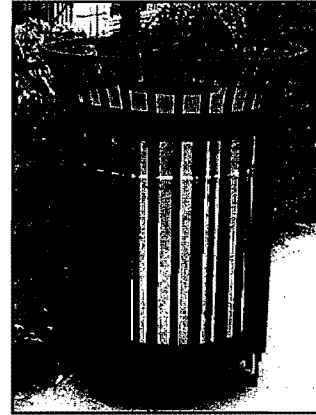
*Metal drinking fountains with fluted poles help define the character of the Historic District.*

### Drinking Fountains

4.6.4 Drinking fountains should be universally accessible.

4.6.5 Drinking fountains should be cast metal and painted black, brown, bronze, or verdigris.

4.6.6 Pet fountains should be included at major pedestrian nodes.



*Multiple, mismatched publication racks create clutter and detract from the historic character of the District.*



*Unified publication racks promote the historic character of the District.*

### Publication Racks & Kiosks

4.6.7 Publication racks should be consolidated, with no more than two per block, placed so as not to interfere with building access or the accessible route of travel.

4.6.8 Informational kiosks should be located at major pedestrian nodes.

4.6.9 Publication racks and kiosks should be wood or metal painted black, brown, bronze, or verdigris.



## 4.7 LOCATIONS FOR ART



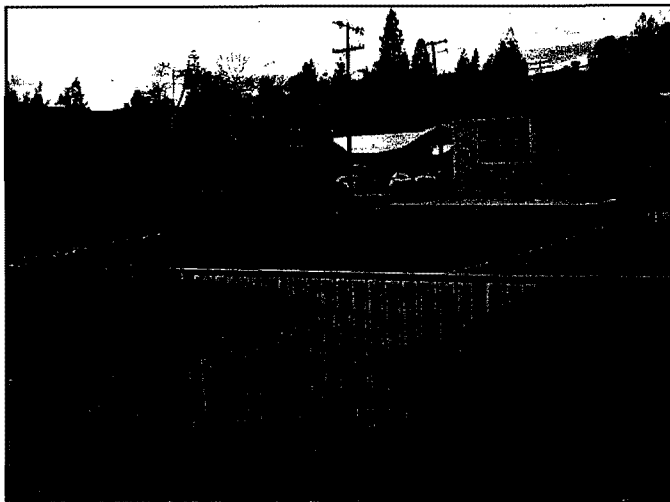
4.7.1 Public art themes should focus on historical, cultural, or community identity.

4.7.2 A variety of display spaces should be reserved for art, including medians, store fronts and public plazas.

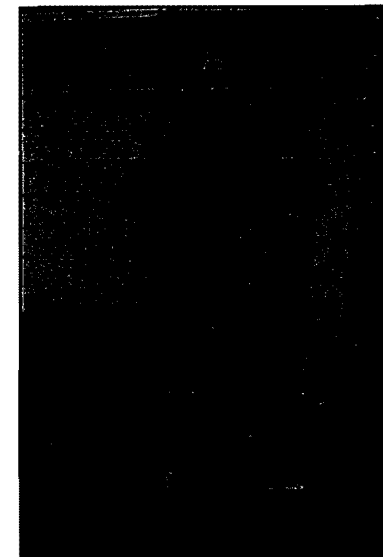
4.7.3 The work of local and regional artists should be celebrated.



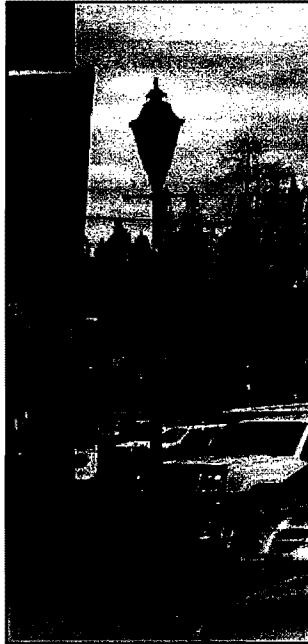
*A number of historically-themed sculptures are currently found in the Historic District.*



*This existing park may be a good location for a public art piece to create a strong park identity.*



## 4.8 LIGHTING



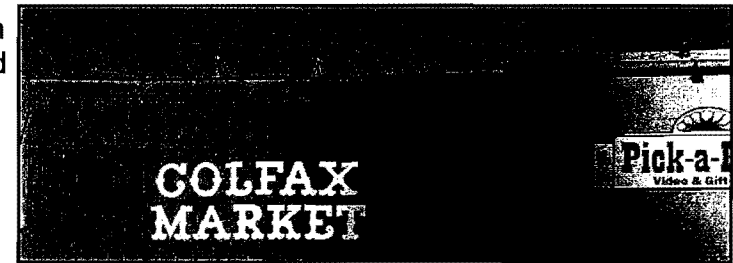
*Existing streetlight style..*

### Street and Path Lighting

- 4.8.1 Install lighting to ensure safety of pedestrians and vehicular traffic.
- 4.8.2 Streetlight poles should be metal and typically fluted with decorative base, finials, and arms consistent with the existing streetlight in the Historic Core.
- 4.8.3 All park or path lighting in a given area shall be of a consistent historical style and shall blend with the existing streetlight style in the Historic Core.
- 4.8.3 Traditional materials and finishes, such as cast iron, baked enamel, or oxidized copper should be used.
- 4.8.5 Scale lights to the intended purpose and include dark-sky light cut-off features to preserve the rural night sky of Colfax.

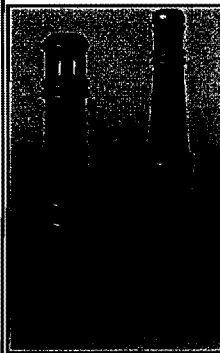
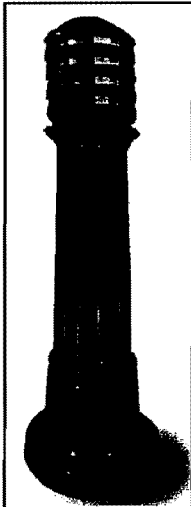


*Existing streetlight style..*



*Lighting used to highlight signs should be discrete.*

- 4.8.6 Lighted bollards may be used to illuminate and control access to pedestrian paths.



*Lighted bollards in appropriate historic style.*



### Building Lighting

- 4.8.7 Lighting may be used on buildings to highlight architectural features, illuminate signs, and provide safety lighting for building users.
- 4.8.8 Visible lighting fixtures should maintain the style of the building they illuminate.
- 4.8.9 Seasonal temporary accent lighting ("Christmas lights") may be used during the holiday season

# GLOSSARY

**Arcade:** A covered passage or walkway support by arches or vaults.

**Awning:** A canvas cover attached to the side of a building by a wood or metal frame, typically collapsible.

**BOC:** Back of curb

**Cornice:** Horizontal element at roofline often projecting over façade to shed rain and ornamented.

**Façade:** The exterior walls of a building.

**Finial:** An architectural detail to emphasize the tip of a fence post, light pole, gable, or other linear element. Often shaped as a spearhead, ball, eagle, or rod.

**Historic Structure:** A structure listed on or eligible to be listed on the Federal, State, or local Register of Historic Places. Typically, a structure must be over 50 years old to be eligible for listing on the Federal Register of Historic Places.

**Loggia:** A covered passageway open on one side with arches or large windows.

**Modular Paver:** A paving unit used to create a walkway, road, or other ground surface such as cobblestones, bricks, or concrete paving stones.

**Parapet Wall:** The section of a façade that rises above the roofline; typically only on the front of a building, but in some instances around the entire perimeter, particularly on flat-roofed buildings.

**Pergola:** A structure of pillars and overhead beams shading a walk or patio.

**Pointing (brick):** The style of joint used between bricks. This refers to the size and depth of joint.

**Ramp:** A walkway with a slope 5% or greater, not to exceed 8.33% (1 foot vertical to 12 feet horizontal).

**Spanish Revival:** Architectural style popular from 1910 to 1930

**Traffic Calming:** Strategies to slow down vehicular traffic, producing a safer environment for pedestrians and bicyclists.

**Trellis:** A structure on which to train climbing plants; it may be attached to a building or free standing.

**Verdigris:** The color of the blue-green patina formed as copper, brass or bronze ages.

## **APPENDIX A**

### **CEQA and the California Register of Historical Resources**

Significant architectural, archeological, historical and cultural resources are all protected from potential adverse change by the California Environmental Quality Act. While there are various criteria by which individual resources may be deemed significant, one of the most common is eligibility for listing in the California Register of Historical Resources.

A historical resource may be listed in the California Register if it meets any of the following criteria:

- (1) it is associated with events that have made a significant contribution to the broad patterns of California's history and cultural heritage;
- (2) it is associated with the lives of persons important in California's past;
- (3) it embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of an important creative individual, or possesses high artistic value; or
- (4) it has yielded or is likely to yield information important in prehistory or history.

The Register also includes properties which are listed or have been formally determined to be eligible for listing in the National Register, State Historical Landmarks, and eligible Points of Historical Interest. Inclusion in the Register may also result from nomination by local communities for resources that contribute to the significance of a local historic district, individual historical resources, historical resources identified in historic resources surveys conducted in accordance with State Historic Preservation Office (SHPO) procedures, historic resources or districts, and local landmarks or historic properties designated under local ordinance.

## **APPENDIX B**

### **U.S. Secretary of the Interior's Standards for Rehabilitation**

- A property will be used as it was historically or be given a new use that requires minimal change to its distinctive materials, features, spaces, and spatial relationships.
- The historic character of a property will be retained and preserved. The removal of distinctive materials or alteration of features, spaces, and spatial relationships that characterize a property will be avoided.
- Each property will be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or elements from other historic properties, will not be undertaken.
- Changes to a property that have acquired historic significance in their own right will be retained and preserved.
- Distinctive materials, features, finishes, and construction techniques or examples of craftsmanship that characterize a property will be preserved.
- Deteriorated historic features will be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature will match the old in design, color, texture, and, where possible, materials. Replacement of missing features will be substantiated by documentary and physical evidence.
- Chemical or physical treatments, if appropriate, will be undertaken using the gentlest means possible. Treatments that cause damage to historic materials will not be used.
- Archeological resources will be protected and preserved in place. If such resources must be disturbed, mitigation measures will be undertaken.
- New additions, exterior alterations, or related new construction will not destroy historic materials, features, and spatial relationships that characterize the property. The new work will be differentiated from the old and will be compatible with the historic materials, features, size, scale and proportion, and massing to protect the integrity of the property and its environment.
- New additions and adjacent or related new construction will be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

# **Part B: City of Colfax Historic Core Property Analysis**

**City of Colfax**

# **Historic Core Property Analysis**

October 2009

Prepared for:  
City of Colfax



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ENVIRONMENTAL CONSULTING • PLANNING • LANDSCAPE ARCHITECTURE

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### **Appendix A: Historic Core Property Inventory – Occupied**

### **Appendix B: Historic Core Property Inventory – Vacant**



# 1. HISTORIC CORE INVENTORY

## 1.1 Statement of Purpose

The focus of this inventory and assessment are the portions of the 34 parcels comprising the Historic Core within the Colfax Historic District overlay zone (see Figure 1 and Figure 2). The inventory was conducted to catalogue the current development status of the parcels in the Historic Core as a framework for developing strategies to maximize the economic potential of the area. While the detailed inventory included only parcels in the Historic Core, it is expected that stimulating economic activity in the Historic Core will also help provide an economic foundation for improved commercial and retail activity in the larger Historic District. It will also be important to coordinate the economic vision for the Historic Core with improvements to the undeveloped parcels or renovations to structures in the Historic District outside of the Historic Core. New businesses in the larger Historic District should complement the Historic Core businesses by offering goods and services that expand opportunities for local residents and visitors without detracting from the vitality of the Historic Core.

The inventory findings provide current descriptions of the development status of each parcel, including numerous building and parcel characteristics, and observations about opportunities and constraints. The individual inventory records are included in *Appendix A – Occupied Property* and *Appendix B – Vacant Property*. Occupied properties are those developed parcels with a structure that is currently in use for either commercial retail or residential purposes in accordance with the zoning designation. Vacant properties include parcels with no structures, or developed parcels with unoccupied structures.

## 1.2 Existing Land Use

Most of the inventoried parcels are adjacent to Main Street, while the remaining parcels are on Grass Valley Street and North Auburn Street. Grass Valley Street roughly bisects the Historic Core from east to west. Main Street (North and South) is the primary north-south arterial through the area. Other significant transportation infrastructure in area includes the rail line east of Main Street. All of the parcels are zoned entirely or in part for Commercial Retail (C-R) uses. Portions of two parcels are zoned Light Industrial (I-L), however these portions are outside of the Historic District overlay zone.

### **Historic District Overlay**

The Historic District is established by municipal code as an overlay zone. As such, it adds direction on development and land use activities to that provided by



Colfax Historic Core Building Facades

the underlying zone designation. The purpose of the Historic District overlay zone is to maintain the historic resources of Colfax while also enhancing the city's character and visual appearance. Proposed new development and certain renovations to existing buildings within the Historic District are subject to design review per Title 17, Chapter 17.96 of the Municipal Code, which directs conformance with the Colfax Design Guidelines as a means of preserving these values.

The zoning designations underlying the Historic District overlay include Commercial Retail (C-R), Industrial (I-L), Single-family Residential (R-1-5 and R-1-10), and Multi-family Residential (RM-1 and RM-2). Each of these plays an important role in contributing to the economic vitality of the Historic Core. The C-R and I-L zoned parcels allow for various business enterprises that provide jobs and contribute to the City's sales tax base. The residential zoned parcels provide a local base of customers for these businesses as well as nearby housing for employees. However, since all of the parcels in the Historic Core are zoned for commercial uses, consideration of economic opportunities for the Historic Core must be consistent with the uses allowed under this zoning designation.

### ***Commercial Retail***

The City of Colfax General Plan states the purpose of the commercial retail district (C-R) is "to provide for areas in the center of the city and in other appropriate locations where shopping centers may be established to serve surrounding residential neighborhoods and the outlying districts. The regulations of this district are designed to promote a combination of retail and service facilities to meet the needs of residents of the surrounding area."

The range of permitted uses includes retail stores, service businesses, professional offices, restaurants, bars, theaters, business and technical schools, and parking for passenger vehicles. A single residential living area with no more than five dwelling units is also allowed provided it is attached or incorporated with the commercial structure and is architecturally consistent.

The zoning ordinance also allows for a number of conditional uses. These include public or quasi-public uses, service stations, hotels and motels, meeting facilities, establishments providing live music and dancing, cabinet shops, warehouses, and churches.

The current land uses generally comply with these allowed uses, with the exception of two residences not associated with a business.

### ***Future Land Use and Zoning***

The City of Colfax is currently revising its General Plan and the land use designations for parcels within its Planning Area. The proposed Land Use Map shows the parcels in the Historic Core with a Mixed Use (MU) designation. The Mixed Use designation generally allows a variety of land uses to create a pedestrian-oriented neighborhood in which residents, visitors, and workers can walk to jobs, shops, and services. The implementation of this Mixed Use designation will require changes in the City's zoning code to regulate specific aspects of new development and redevelopment. However, the current land uses in the Historic Core are already pedestrian-oriented and maintaining this characteristic will be an important aspect of economic development for the area. The proposed Mixed Use designation would also eliminate the inconsistency of the existing residential land uses within the Historic Core. Specific policies will be needed as part of implementing the Mixed Use designation to address potential conflicts

between commercial, office, and residential uses. For example, where an outdoor dining use is established, nearby residential uses would need to accept the normally allowed activities associated with dining use, such as noise, odors, and public assembly.

### *1.3 Improved Parcels*

Thirty-two of the 34 parcels in the Colfax Historic Core are improved, some with multiple structures or buildings that house multiple businesses. These improved parcels include three that are used as public open space, and three other parcels used for City Hall, the Colfax Library, and the Colfax Train Station. There are three parcels used for two residential structures, one of which is currently unoccupied. The remaining parcels have commercial buildings, of which six were vacant and two were partially occupied at the time of the inventory (January 30 and February 10, 2009).

At the time of the inventory there were approximately 68,900 square feet of commercial retail building space in the Historic Core.<sup>1</sup> Of this, 17,180 square feet or 25% of commercial retail space was unoccupied. Another 10,272 square feet or 15% of commercial retail space was partially occupied. Approximately 41,407 square feet or 60% of commercial retail space was fully occupied. Subsequent relocation of one business and establishment of a new business have revised these numbers to 18% unoccupied, 11% partially occupied, and 70% fully occupied as of October, 2009.

By comparison, the retail vacancy rate for the Roseville/Rocklin area in the fourth quarter of 2008 was 12.3%. For the Auburn/Loomis area the retail vacancy rate for the same period was 8.7%, and in Placerville it was 4%.<sup>2</sup> In Truckee, the retail vacancy rate in January 2008 was estimated to be 10 to 12%.<sup>3</sup> This suggests that the Colfax Historic Core may be less successful at attracting and maintaining retail occupants than these other communities. Retail vacancies detract from the community due to reduced retail revenues and sales taxes. Empty retail spaces also diminish the sense of vitality and viability in the Historic Core which makes the area less appealing to visitors. Potential new retailers can also be driven away by high vacancy rates because they suggest the area is not a desirable shopping location.

### *1.4 Unimproved Parcels*

There are only 2 parcels in the Colfax Historic Core that are not improved and remain vacant. These are both very small parcels (.07 and .03 acres) with limited retail development potential. One appears to provide ingress and egress to a residential property without street frontage. The other is located at the corner of Depot Street and North Main Street. However, the 4.71 acre parcel where Colfax Feed & Hay is currently located does have some vacant space that could accommodate additional structures.

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<sup>1</sup> Building square footage as provided by Placer County Assessor database.

<sup>2</sup> Colliers International, "Sacramento/Roseville Retail Outlook: Market Research", February/March 2009.

<sup>3</sup> Bay Area Economics, "Inclusionary Housing Policy Pro-forma Analysis – Commercial Projects", Final Memorandum to the Town of Truckee, February 29, 2008.

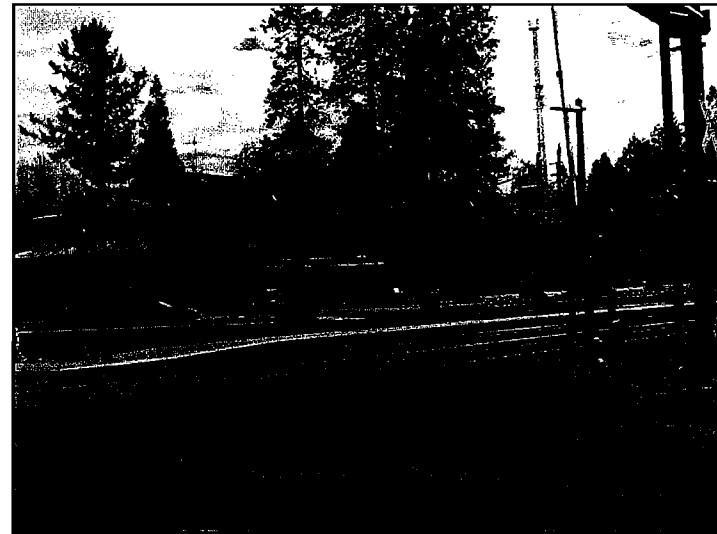
## 1.5 Streetscape Condition

The City of Colfax Economic Development Report and Strategy (2002) identifies the development of a pedestrian friendly streetscape in the historic downtown with consistently appealing visual character as one of four proposed components of the tourism development program.

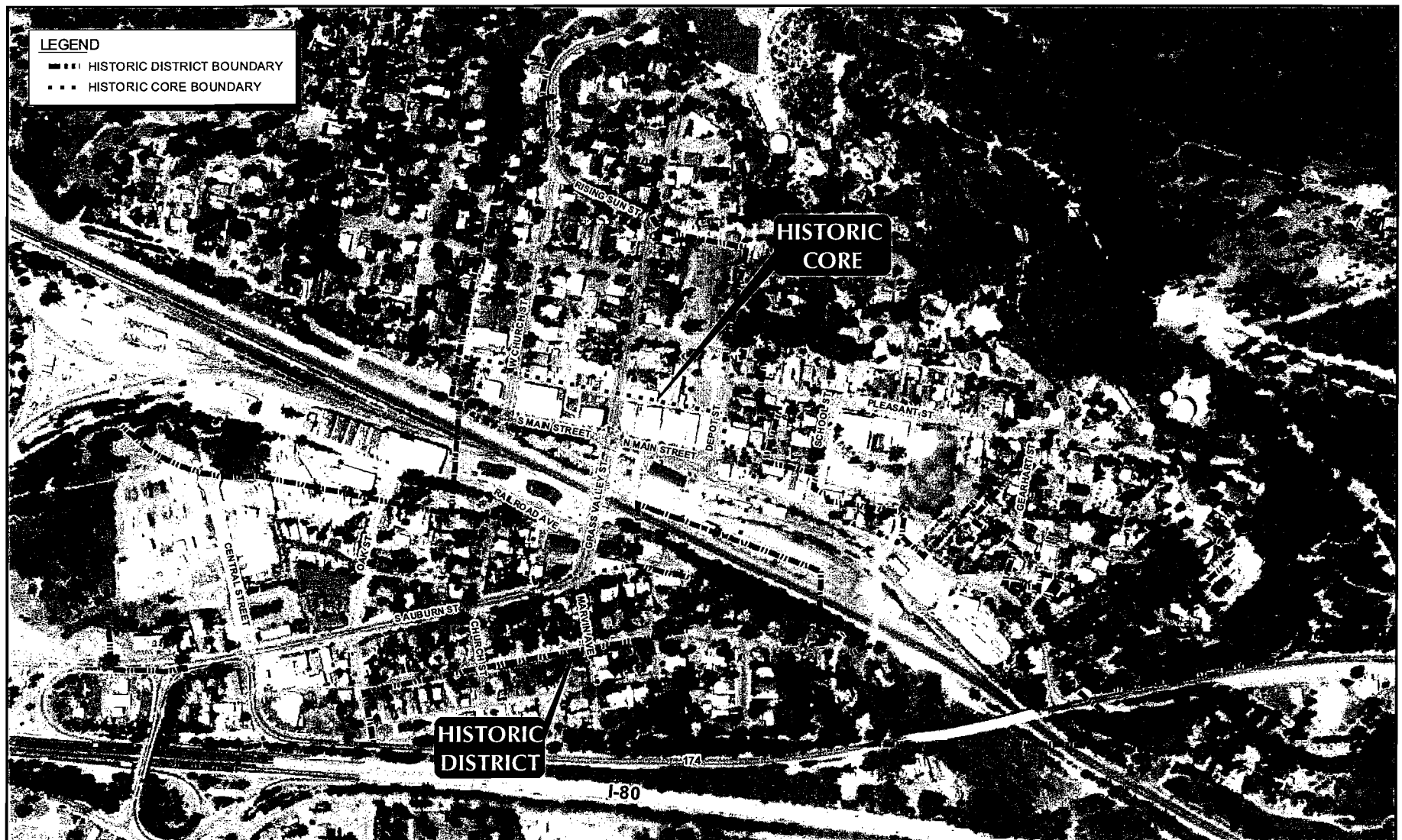
Currently, the most unifying features of the downtown streetscape are the historic character and scale of the buildings. Buildings are one or two stories tall, and many have features that invoke the architectural vernacular of the late nineteenth and early twentieth centuries, including corrugated metal overhangs, false fronts, and period windows, doors, and masonry. A single style of streetlight standards is used throughout the downtown area and is consistent with the architectural styles. Many of the signs in the downtown area are also historic or use lettering styles that convey an historic quality.

Other streetscape elements detract from the cohesive appearance of the downtown area. Parking spaces are both parallel and angled. Curbs and sidewalks provide separation between parking and pedestrians in most areas, but the condition of the curbs and sidewalks is variable. Access for people with disabilities can be challenging in certain areas depending on the configuration of parking spaces. Plantings are located at several major corners and a few mid-block bump-outs. Some shops have additional pots or barrels with plantings in front. There is no clear pedestrian crossing at the railroad tracks on Grass Valley Street. Relatively few shops have adequate sidewalk frontage to allow outdoor seating, but there are several types of benches scattered throughout the area along with other site furnishings such as trash cans.

Developing a more consistent approach to the design of these streetscape elements will help visitors recognize the boundaries of the downtown Historic Core area, and facilitate pedestrian access. Getting people out of their cars and encouraging a pleasant walking experience through the downtown Historic Core will increase the likelihood of visitors patronizing multiple shops and enhance the destination quality of Colfax.



Grass Valley Street Crossing



## CITY OF COLFAX HISTORIC DISTRICT OVERLAY AND HISTORIC DISTRICT

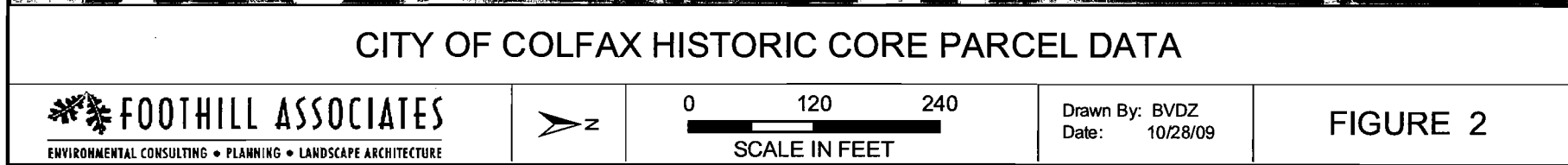
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0 400 800  
 SCALE IN FEET

Drawn By: BVDZ  
 Date: 10/27/09

**FIGURE 1**



## 2. ECONOMIC STRENGTHS

The City of Colfax Economic Development Report and Strategy identified several factors which support economic development within the Colfax Historic Core as it is currently configured. While these factors alone cannot generate the level of retail and commercial activity envisioned by the City, these qualities are important to recognize so they are not compromised or lost in future redevelopment or enhancements of the district.

### ***Business Mix***

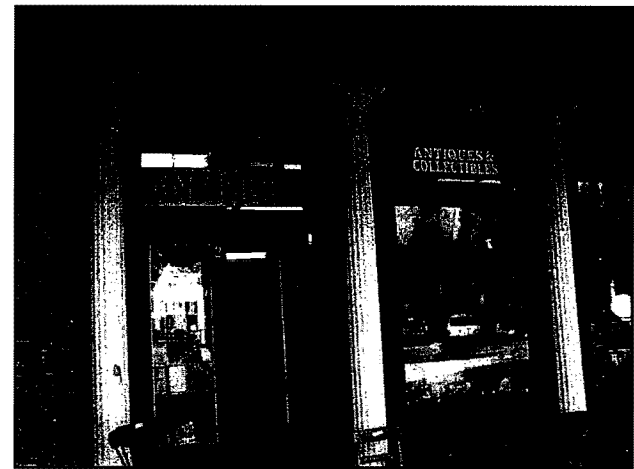
The Colfax historic downtown includes a diverse mix of retail and service businesses. This variety is important because visitors are more likely to patronize a commercial area if they know they can accomplish multiple purchases in one trip. The diversity also provides opportunities for local services as well as those more oriented to tourists. This gives the downtown a feeling of being a vital part of the community, which further encourages the sense of place and uniqueness so important to creating a destination identity.

### ***Historic/Scenic Quality***

Downtown Colfax has retained much of its late nineteenth – early twentieth century architectural charm, and is set amid scenic vistas of a beautiful Sierra landscape. Artifacts throughout town as well as the existing train station serve as reminders of the City's historic connections to the railroad, mining, and the early days of California. The authenticity of the architecture, rich local history, and the scenic setting, offer great potential to make downtown Colfax a destination for antiques collectors, California history enthusiasts, and sightseers.

### ***Regional Location***

Colfax is ideally situated on Interstate 80 between the Sacramento metropolitan area to the west and the destinations of Reno and Lake Tahoe to the east. Interstate 80 carries both vacation and business travel on a year-round basis. Colfax is the last opportunity for eastbound travelers heading from the Bay Area, Central Valley and the Sierra foothills to find a meaningful selection of goods and services until they reach Truckee, and it is the first community westbound travelers encounter after descending from the Truckee/Tahoe area. Colfax is often below the snowline which enhances its attractiveness as a place to refuel, purchase food, or take a break for people traveling the I-80 corridor especially during winter snowstorms.



Historic Architecture in Downtown Colfax

### ***Railroad Legacy***

The historic Southern Pacific Passenger Depot in downtown Colfax was built in 1905 and is still a stop on the California Zephyr line connecting the San Francisco Bay Area and Chicago. One westbound and one eastbound train depart from the station daily, which serves as a waiting area for arriving and departing passengers. Restoration of the station, accomplished through the collaboration of the City of Colfax, the Colfax Area Historical Society, and the Placer-Sierra Railroad Heritage Society was recognized with the Governor's Historic Preservation Award in 2006. The station is also the home of the Colfax Historical Museum, the Visitor Information Center, and the Chamber of Commerce.

Other railroad themed elements in the historic downtown, such as the Freight Depot and the restored Santa Fe coach car, remind visitors of the crucial role played by Colfax in the early establishment of the transcontinental railroad. These features support economic vitality by attracting visitors who are interested in railroad history and by providing additional charm, visual interest, and a unifying character to the downtown area. Additional improvements to the Historic Core should draw from railroad inspired design concepts to strengthen this element.



Historic Freight Depot Built in 1880



Downtown Sidewalks and Crossings

### ***Pedestrian Scale***

Because much of the original street layout of the historic downtown has been retained, the area supports a very pedestrian-oriented experience. The scale of the buildings, street widths, and intersections are well-suited to encourage casual strolling and exploration. Separation between vehicular and pedestrian routes is good in most areas, and the posted travel speed for vehicles is compatible with frequent pedestrian crossings. These features make the historic downtown area feel safe for pedestrians, especially for families with children and the elderly. The economic benefit of these pedestrian-oriented qualities is that visitors are encouraged to get out of their cars and move through the downtown area at a leisurely pace, discovering a variety of shopping and dining opportunities, potentially extending their stay, and visiting more businesses.

### ***Community Ownership***

The overwhelming majority of businesses operating in the Historic Core are locally owned and operated. There is very little evidence of the commercial homogenization that has destroyed the unique character of so many small American towns. While the careful inclusion of some popular national retailers could successfully be accommodated in the



business mix, it will be important to retain this sense of local character that gives visitors a reason to come to Colfax for a different type of shopping experience than they can get in their own communities. This can be accomplished through thoughtful design of buildings and signage to preserve the scale and visual qualities that give the downtown area its historic character.

### ***Regional Event Center***

The City of Colfax and the Colfax Chamber of Commerce host a number of popular annual events and festivals that attract attendance from the larger region. Events such as the Railroad Days Festival and Business Fair, Colfax Winterfest, and Colfax Independence Day bring in visitors to the Historic Core who then shop in other areas of the city as well. The repetition of these events each year helps create repeat visitation and gives people a reason to schedule their future travel plans around these events. The downtown Historic Core is well-suited to hosting these events, which are enhanced by the charm and pedestrian-oriented scale of the area. These events are also popular among regional residents because they represent a chance to enjoy a significant entertainment or recreation experience as part of a relatively low-cost day trip within a short driving distance of their homes.

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### 3. ECONOMIC CHALLENGES

The economic potential of the Colfax Historic Core has not been realized due to several factors which impair visitation and discourage new business development. Many of these factors can be addressed through various design and programmatic measures.

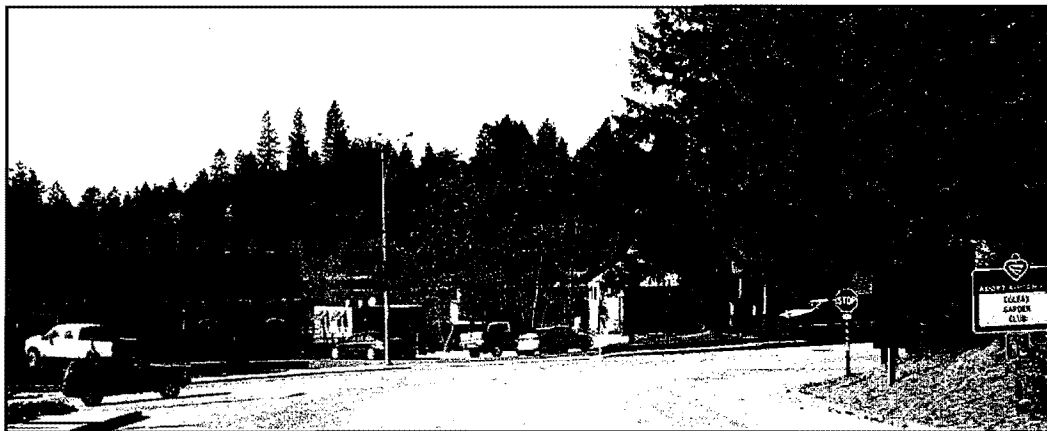
#### ***Lack of Visual Connectivity to I-80 Corridor***

Thousands of travelers pass through Colfax each day on Interstate 80 with no awareness of the City's charming historic downtown area. Unlike other Sierra communities such as Placerville, Auburn, Grass Valley, Nevada City, and Downieville, there is no visual connection between the highway and the heart of the City. The overwhelming visual impression of Colfax from Interstate 80 is formed by the mix of frontage road businesses and billboards flanking the freeway. Travelers who do exit the freeway in Colfax do so mainly to refuel themselves or their cars at the business located within the highway corridor.

The traffic and circulation patterns within the highway corridor further confound visitation to the historic core. Northbound motorists exiting the freeway find themselves on South Canyon Way approaching the intersection with South Auburn Street. Southbound motorists exit directly onto South Auburn Street. Neither location provides a clear view of the downtown, and there is no connectivity to a major arterial road that would suggest a significant destination lies ahead. Signage



Entering Colfax on Interstate 80



Signage at South Canyon Way and South Auburn Street

in both locations is confusing consisting of various sizes and styles trying to direct motorists to highway corridor businesses and downtown while routing egress and access to local streets, Highway 174, and the freeway. The Historic Core remains a hidden jewel that is likely to be discovered only by those whose sense of curiosity takes them away from the highway business corridor.

### ***Limited Destination Opportunities***

Several seasonal festivals are held each year in Colfax but these alone do not provide the sustained visitation needed for economic stability. There are many wonderful features in Colfax and the surrounding area that could form the basis for developing destination opportunities to inspire year round regional visitation. However, until these opportunities are developed and publicized, most travelers passing through Colfax will be on their way to other destinations. Such visitation will continue to emphasize patronage of highway corridor business rather than businesses in the Historic Core.

### ***Limited Transit Options***

Even though the Historic Core occupies a relatively small area and is inviting for pedestrian uses, visitors to Colfax who arrive on the train have few transportation options to get around town or to outlying areas. There are no local taxis or shuttle services to take visitors from the train station to accommodations or other destinations. There is also a shortage of parking spaces configured for buses or recreational vehicles (RVs). This makes downtown Colfax an less attractive place for tour buses and vacationers traveling to/from the Sierra to stop for a meal and shopping break.

### ***Commercial Building Mix***

Most of the commercial buildings in the Historic Core are between 1,000 to 5,000 square feet in size. Several larger buildings are broken into multiple retail and office spaces. While the relatively small size of the buildings contributes to the pedestrian oriented scale of the area, it does impose limitations on the types and size of businesses that could be operated within these structures. Because smaller enterprises may find it difficult to compete on price alone with retailers providing similar products on a larger scale, other incentives will be needed to attract shoppers to the Historic Core. These could include a unique or more enjoyable shopping experience as well as access to a wider variety of shopping and dining opportunities in close proximity.

### ***Age of Buildings***

Modifications to the buildings in the Historic Core are limited by their protected, historic character. However, potential business owners may still need to make periodic structural, electrical, and/or plumbing renovations to support their operational needs and public safety. Business owners who are considering locating in the Historic Core will need to be aware of these maintenance and preservation requirements and associated costs.



Partially Renovated Historic Colfax Hotel next to Train Station

## 4. RECOMMENDATIONS

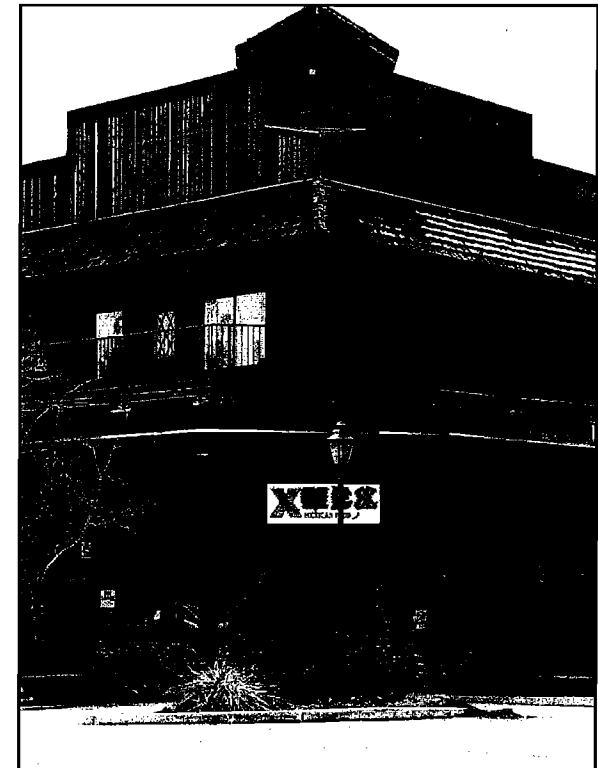
Downtown Colfax merchants have the potential to serve two major markets. The local market area includes Colfax proper and the unincorporated areas between the communities of Weimar, Dutch Flat, and Chicago Park. The regional market includes people traveling along the Interstate 80 corridor between the greater Bay Area and Sacramento urban areas and the Tahoe and Reno areas. In order for Colfax businesses to successfully attract customers from either market, they must offer goods, services, and experiences that are competitive with those available in other communities in the area, such as Loomis, Auburn, Grass Valley, and Nevada City.

The following recommendations are intended to enhance the retail appeal of the Colfax Historic Core, and by extension overall Historic District, as a shopping choice for both local and regional markets. A more comprehensive review of economic development strategies for the larger commercial area of Colfax is provided in the City of Colfax Economic Development Report and Strategy.

### 4.1 Business Mix

It is important for the downtown Historic Core area to provide a variety of complementary businesses that will serve both local residents as well as visitors, and provide employment opportunities for local residents. The diversity inherent in meeting these three objectives will create more stability throughout the business cycle as merchants can serve local residents during times when seasonal or weekend tourism is low. The appeal of the downtown area to visitors will also be enhanced if they have a sense that the businesses are part of a vital, local community that has an authentic character and charm instead of a theme park quality fabricated to attract tourists.

The Colfax Strategic Economic Base Analysis estimated there were nearly 4,000 households in the local service area in 2001. Based on projected purchases per household and estimated sales for Colfax businesses, only 58% of demand is being met by local establishments. Certain retail segments, such as restaurants (87%) and automotive (91%), captured a relatively high number of local sales. These segments also appear to have potential for expansion to serve non-local demand for services. Automotive services would typically be clustered in the highway corridor while restaurants could be either downtown or in the highway corridor. Development of a destination restaurant featuring local products in the Historic Core, could increase visitation to Colfax as well as strengthen sense of a local identity. Visitors drawn by such a destination feature would also be more likely to patronize



Combination Office and Retail Space

other businesses, such as the theater, that on their own might not provide sufficient reason for people to come to Colfax.

Other segments with the greatest potential to expand or attract new sales were identified as apparel stores, general merchandise, specialty retail, and building materials and home furnishings. Several of these would be appropriate for the Historic Core area. These could include specialty clothing and retail shops that also sell to a larger market through mail orders or the web. General merchandise stores, such as Walgreens or CVS, would most likely not locate in the Historic Core due to building size and parking limitations. However, home furnishing shops featuring local crafts, custom fabrication, and artisan products would be appropriate for the Historic Core.

Developing the correct mix of goods and services needed to serve both local and visitor markets should be an objective for individual merchants as well as for the downtown area as a whole. For individual merchants, this means diversifying their inventory and services to address the demands of local shoppers while also including products that will appeal to visitors. For example, a store that provides a lunch counter and groceries primarily for local residents could also carry specialty local products and crafts that would appeal to visitors.

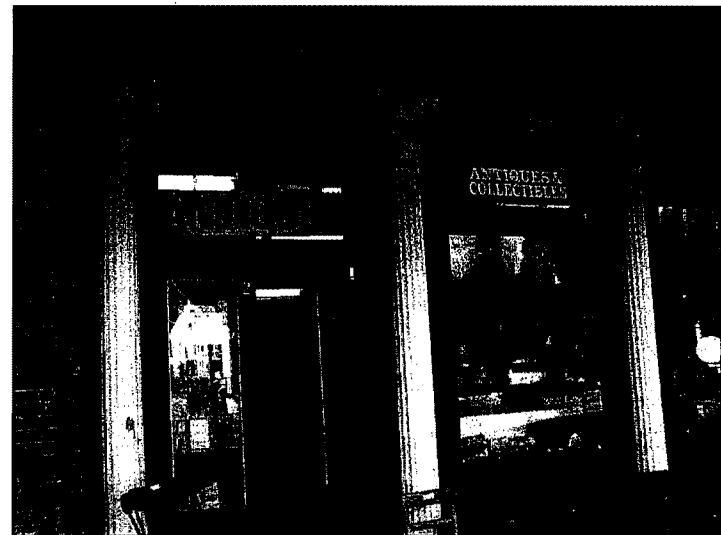
The different types of businesses located in the downtown area should complement each other by limiting duplication of specific goods and services while collectively meeting demands of local residents and travelers. The business mix should include retail, office, and compatible light industrial uses so the overall dynamic of the downtown area is not exclusively dependent on any one sector. In addition, these land uses will help support each other. Office and manufacturing workers will utilize retail services during their work day, and employers will be more attracted to setting up businesses in an area where employees do not have to travel far for lunch breaks or errands. Specific recommendations for new business sectors are provided below.

## **4.2 Business Sectors**

Serving local residents must also be accompanied by increased visitor spending in Colfax in order to stimulate economic and job growth. The City of Colfax Economic Development Report and Strategy shows that the percentage of taxable sales derived from visitor spending in Colfax is far below that of Placer County overall. The main issues appear to be lack of overnight lodging and limited appeal as a day visit destination. Development of the following business sectors would support both of these objectives, while also providing jobs, goods, and services to the local community.

### ***Niche Retail***

As detailed above, there appears to be demand for specialty retail products if potential shoppers can be drawn to the Historic Core area. Shops that offer unique, high quality products such as jewelry, gifts, and clothing together with personal service and inviting ambience would enhance the identity of the Historic

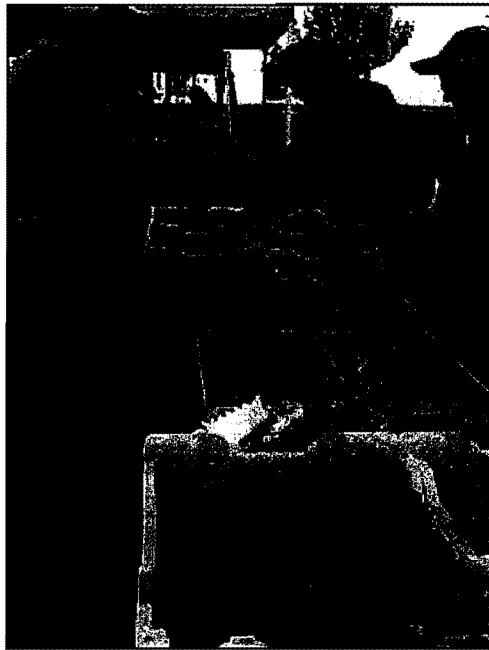


Specialty Products Enhance Destination Appeal

Core as a shopping destination. This combination of niche retail offered in a scenic historic setting has proven to be successful in attracting shoppers to other Gold Rush communities such as Placerville, Grass Valley, and Nevada City, where visitors stroll from shop to shop in addition to patronizing local restaurants and services.

### ***Local Products***

Goods that incorporate local craftsmanship and materials would provide shoppers with a unique reason to visit Colfax, and reinforce the sense of identity of the community. The range of possible products is extensive, including locally grown or manufactured foods, crafts, artwork, furniture, clothing, fabrics, yarns, body care products, and musical instruments. Such products could be sold in shops year round to features local enterprises and also in seasonal Farmer's Markets or Crafts Fairs.



Locals and Tourists Visit Farmers Markets

### ***Destination Hotel/Conference Center***

Businesses in the Historic Core would greatly benefit if visitors could enjoy overnight accommodations within easy walking distance, especially at a hotel with its own scenic and historic charm. The inclusion of spa, concierge, and other resort-oriented services would enhance the destination quality of such a place. The hotel could also provide guests with information about where to enjoy local day trips for sightseeing and outdoor recreation, as well as local events. A destination hotel would increase the number of people who visited Colfax and stayed for several days, taking advantage of local merchants and services during their visit. Such a facility could also be used as a conference center for small events or as a convenient setting for retreats and workshops by businesses in the greater Bay Area and Sacramento regions. The City of Colfax Economic Development Report and Strategy suggests that 40-room hotel with 3,000 square feet in conference facilities would be feasible in Colfax. Both conference attendees and overnight visitors would generate local sales in excess of the facility fees due to purchase of meals, and other goods and services associated with their stay.

### ***Bed & Breakfast Accommodations***

Bed & Breakfast establishments located in proximity to the Historic Core could also provide some of this accommodation capacity, and would offer another type of vacation lodging option. While most of the historic homes in the larger Historic District are not in the Historic Core, they are within an easy walking distance to the retail shops and could be an attractive alternative to the hotel experience.

### ***Outdoor Recreation***

There are numerous opportunities for outdoor recreation in the Colfax area, but relatively few retail shops or promotional efforts targeting this population in any coordinated way. In the same way that

the City of Auburn has established an identity for itself as the Endurance Capital of the World, Colfax could be promoting itself as a gateway to Sierra recreation. Colfax is an ideal location to serve as a base camp for wonderful day trips featuring bicycling, hiking, horseback riding, fishing, rafting, and skiing. Visitors coming to Colfax for this purpose would patronize a wide variety of businesses to purchase food, clothing, equipment, guide services, and accommodations. Many of these businesses would be ideal for location in the Historic Core, where the architectural charm and historic character would compliment visitors' enjoyment of the local recreational opportunities. Since many outdoor recreation enthusiasts engage in their activities on the weekends, this would also provide an impetus for businesses to remain open on Saturday and Sunday to serve this population.

### **4.3 Destination Opportunities**

Development of the destination potential for Colfax is challenging because it will require a combination of public and private investment in a coordinated effort. The success of promoting Colfax as a destination is also likely to be greater if multiple themes are targeted in a manner that provides synergistic benefits. Possible features that the City could focus on in developing its destination identity include the connection between the City and the railroad, and recreational opportunities.

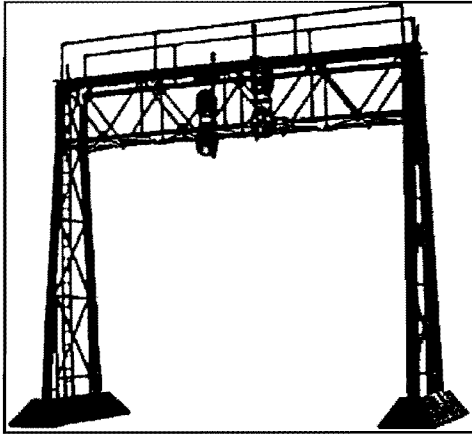
#### ***Railroad Museum/Transportation Center***

The relationship between Colfax and the railroad sets the City apart from the other historic Mother Lode towns of Sutter Creek, Jackson, Placerville, Nevada City, and Grass Valley. These communities have developed their tourist destination image based largely on their mining origins. A focus on the railroad would help Colfax differentiate itself from these other destinations. There is very strong support within the community for development of a Railroad Museum in Colfax. The continued presence of operating passenger and freight trains through the City, and the historic railroad structures lend additional impetus to this idea. A Railroad Museum would also complement the other historical interpretive efforts in the City, such as the Colfax Area Heritage Museum, and the various events coordinated by the Colfax Area Historical Society.

A Railroad Museum could be located near the Historic Core area and would provide a strong attraction for visitation to other businesses in the area. The Railroad theme could be reiterated in the design of the streetscape improvements, such as gateways, monuments, benches, and signage. Operation of the Museum would provide local employment, as well. The Museum could feature artifacts, interpretive displays, educational workshops, and special events focusing on the development of the railroads, their role in local history, technical aspects, and related folk lore.







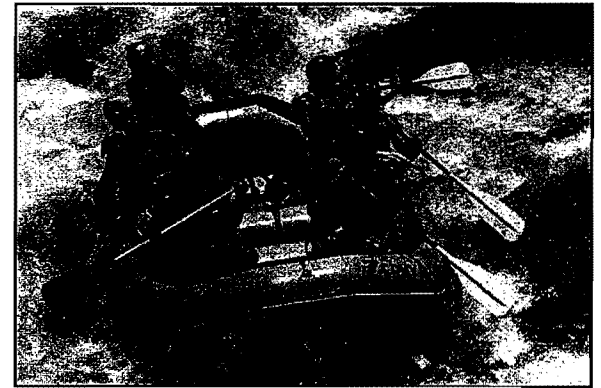
Reclaimed railroad signal bridges could be incorporated in streetscape improvements as gateway features.

The design of the Railroad Museum could also be integrated with space to provide transit center capacity for buses, tour vehicles, and shuttles to recreation destinations, building on the identity of Colfax as a transportation hub.

### ***Colfax Recreation Resource Center***

Colfax is ideally located to provide a gateway to Sierra recreation for the greater Bay Area and Sacramento regional markets. If a Colfax Recreation Resource Center were established in Colfax, it could serve as multi-purpose facility encouraging recreation tourism while selling goods and support services. Shuttles could transport visitors from the Center to trailheads for cycling, rafting, and hiking, or for day trips to the ski slopes. Center employees could also provide day trip planning, workshops, and guide services. The Center could also include interpretive displays and hold events to educate visitors about recreation safety, local history, and environmental resources.

The presence of such a facility would help create demand for goods and services provided by local recreation outfitters, as well as increase demand for restaurants and lodging. In order for a Recreation Resource Center to become a reality, it will be important to start building local and regional awareness of Colfax as a center for outdoor recreation as soon as possible. For example, the Colfax Museum and Colfax Visitor Center could provide more recreation information to travelers to encourage visitation for this purpose. The City could also devote part of its web site to promoting recreational resources in the area, perhaps in collaboration with other public and private recreation providers in the area.



Rafting on local rivers is a popular recreational activity enjoyed by many regional visitors.

### ***Special Events and Festivals***

The Historic Core is already the site for several major festivals each year in Colfax, including Winterfest, Colfax Independence Day, and the Railroad Days Festival. Tremendous local community support is evident in the coordination and attendance of these events. Local businesses, the Colfax Area Chamber of Commerce, and Colfax Pride, Inc. are also working to develop other events to attract tourism, such as the Hot Rod Show, the Cruise Ins, and the Colfax Cider, Wine and Food Tour. However, given the proximity of Colfax to the major population centers in the Central Valley and Bay Area, there is potential to attract more visitors to the community for these and other events such as concerts and theater productions if suitable venues can be identified. Event scheduling and promotion also needs to be coordinated so that Colfax events are not competing for visitation on the same days as other regional events. The nature of events and festivals should be unique to Colfax in some

significant way, either related to location, history, local resources, or special community interests. Many of these events could be centered on the Historic Core or at least designed to increase visitation to the Historic Core.

#### 4.4 Job Creation

The two ways to create new jobs in Colfax are either to attract new businesses to the area or to encourage local entrepreneurs and grow the capacity of existing businesses. This latter approach, known as Economic Gardening, has been shown to be successful in many small communities in the Western U.S.<sup>4</sup> Economic Gardening is recognized as a more sustainable approach to job growth because it does not rely on providing tax breaks or financial incentives to lure outside companies to a community. Instead, resources are spent to provide assistance to local business owners who are already vested in the well-being and survival of the community. This assistance often takes the form of better infrastructure, networking support, and market analysis information to help businesses owners succeed.

Colfax is an ideal community for this Economic Gardening approach to job development, especially in the Historic Core due to the limited vacant land, infrastructure constraints, and high percentage of locally owned businesses. The relatively small size of the City and high degree of existing citizen involvement in local government and community events suggests that the collaboration between local government, businesses, professional organizations, and community groups required for this approach to succeed would be feasible. The recommendations above for business mix, business sectors, and development of destination opportunities are also very compatible with the Economic Gardening concept. All of these recommendations focus on leveraging local resources as the foundation for economic and job growth rather than reliance on bringing in new businesses from outside the area.

There are significant resources already available to support local business owners who are currently operating or considering establishing businesses in the Historic Core. The **Colfax Area Chamber of Commerce** provides members with access to marketing opportunities, business development assistance, networking, and community outreach. The **Sierra College Small Business Development Center** offers business management counseling and training through workshops, one-on-one consultation, and online classes. The **Sierra Business Council** makes various events, training, and publications available to local businesses to support sustainable economic development in Sierra communities. **Colfax Pride, Inc.** is a grassroots organization of local residents and business owners who are promoting Colfax through a wide variety of events designed to bring visitors to the City and celebrate the community. The **City of Colfax** has also promoted local economic development at various times through measures including the establishment of an Economic Development Commission, producing a City of Colfax prospectus for potential businesses, and hosting an inventory of available commercial properties on its web site.

For Economic Gardening to succeed in Colfax and the Historic Core, these resources need to be coordinated as part of the overall economic development strategy for the City described in the following section.

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<sup>4</sup> U.S. Small Business Administration, Office of Advocacy, "The Small Business Economy, December 2006: A Report to the President".

## ***4.5 Economic Development in the Historic Core***

The City of Colfax Economic Development Report and Strategy identifies four key components required to build the success of the City overall as a tourist destination. These same four components would also have a positive effect on boosting local sales and ultimately creating jobs in the Historic Core. Each of these components is consistent with the Economic Gardening premise that resources should be focused on building local capacity for business and job development.

### ***Organization***

The first component is organization of community businesses and local government to create a vision and theme for the City of Colfax image. This vision would become the element that would unify all other efforts to stimulate tourism. Development of this vision should be a cooperative endeavor involving diverse interests in the community, and should seek to highlight the unique characteristics of the City, such as the railroad connection; the Historic Core; destination shopping, hotel or dining experiences; and recreation opportunities.

### ***Promotion and Marketing***

Promotion and marketing are identified as the second component. The goals of promotion and marketing are to enhance the name recognition and image of Colfax as a destination for tourism. Promotional activities should occur at multiple coordinated levels, ranging from the City web site to press releases and Chamber of Commerce publications, and should prominently feature the Historic Core as one of the central attractions in Colfax. A comprehensive Marketing Plan should be developed that incorporates these activities as part of an overall Economic Development vision for the City.

### ***Design***

The third essential component is an urban design strategy. The Historic Core has the potential to function as a very strong magnet for destination tourism. Pedestrian-oriented streetscape improvements that would create a more cohesive sense of a place could help encourage visitors to stay longer and explore the area shops and dining options. Several large public spaces that could function both for programmed events and informal gathering would also be helpful. Other beneficial design improvements include better pedestrian, vehicular, and visual connections between the Historic Core and the other areas in the Historic District. Consistent visual and graphic elements of the urban design strategy should also be repeated in the design of signage and promotional materials to reinforce the City vision. This 'branding' of the Historic Colfax character should be a prominent feature of the Marketing Plan.

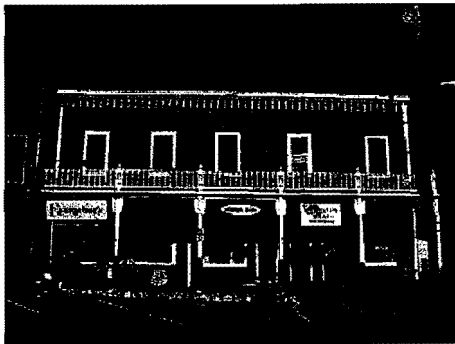
### ***Product Development***

The final element needed to foster a successful tourist destination identity for Colfax is the development of specific tourist oriented 'products' that will draw people to the City, such as events, festivals, accommodations, dining, goods, services, and leisure activities. Recommendations for such products are explored above, and the Historic Core would be an ideal setting for providing many of these products because it is centrally located, has a distinctive, appealing character, and is compatible with pedestrian-oriented design that would encourage visitation.

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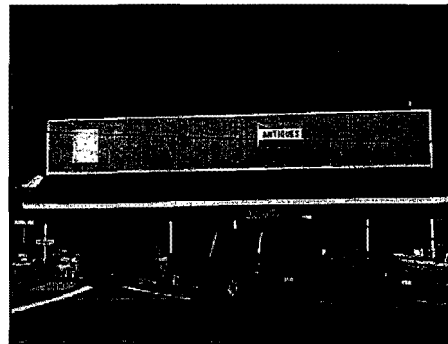
**Appendix A: Historic Core Property Inventory – Occupied**

---



- **APN:**  
006-043-003-000
- **Address:**  
1 — 5 Depot Street  
Colfax, CA 95713
- **Owner:**  
Horst & Ilse  
Krassowski

- **Building Name:** Unknown
- **Current Use:** Evangeline's (#5), InVision Salon (#3), The Real Estate Office, and apartments upstairs
- **Building Age:** Circa 1900
- **Architectural Style:** Eclectic
- **Building SF:** 3,096
- **Improvements/Year Remodeled:** Unknown
- **# Stories:** 2
- **\*Structure Value:** \$221,374
- **Land SF:** .160 acre
- **\*Land Value:** \$62,097
- **Notable Property Features:** Wood siding, railing/balcony, stone wall @ porch, planting area on corner, small planter around telephone pole, diagonal parking—typical.
- **Vehicular Access:** Acceptable
- **ADA Access:** Nearby
- **Opportunities:** Evangeline's and hair salon are major gathering places and anchor location; pedestrian alley in front of porch area.
- **Constraints:** Accessibility; connectivity; parking too close to building—typical; no bike parking area/high corner curb.
- **Notes:** Evangeline's has live music and dinner.



- **APN:**  
006-043-004-000
- **Address:**  
13 Depot Street  
Colfax, CA 95713
- **Owner:**  
Rick & Amy Nichelini

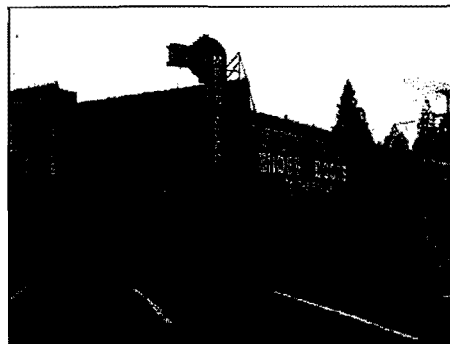
- **Building Name:** Unknown
- **Current Use:** Hair 'n Stuff, Antique Store
- **Building Age:** 1900
- **Architectural Style:** Early Western
- **Building SF:** 1,760
- **Improvements/Year Remodeled:** Recent paint, woodwork and roof overhang is in good condition.
- **# Stories:** 1
- **\*Structure Value:** \$260,000
- **Land SF:** .157 acre
- **\*Land Value:** \$100,000
- **Notable Property Features:** Overhang provides cool shaded walkway and easy access to crosswalk.
- **Vehicular Access:** Acceptable
- **ADA Access:** Acceptable to crosswalk.
- **Opportunities:** Anchor location, next to Evangeline's, pedestrian alley in front, crosswalk to south portion, typical signage on facade above overhang.
- **Constraints:** Parking too close to building; utilities in front of porch area need to be raised/adjusted.
- **Notes:** House located behind businesses—2 hour parking in alley.

\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.



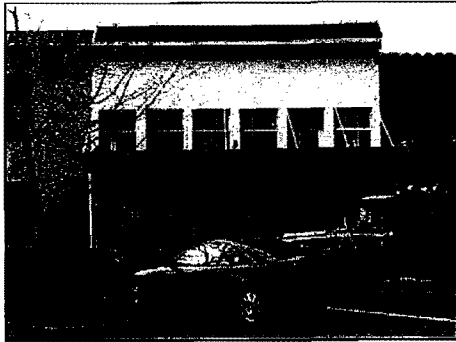
- **APN:**  
006-043-013-000
- **Address:**  
140 Main Street  
Colfax, CA 95713
- **Owner:**  
Union Pacific  
Railroad Company

- **Building Name:** Colfax Feed & Hay
- **Current Use:** Colfax Feed & Hay Warehouse/storage
- **Building Age:** Unknown
- **Architectural Style:** N/A
- **Building SF:** 2,720
- **Improvements/Year Remodeled:** Unknown
- **# Stories:** 1
- **\*Structure Value:** Not available
- **Land SF:** 4.71 acre
- **\*Land Value:** Not available
- **Notable Property Features:** Property has several out-buildings and equipment throughout.
- **Vehicular Access:** Unpaved road
- **ADA Access:** None
- **Opportunities:** Train museum; improve landscape adjacent to Main Street.
- **Constraints:** Unknown
- **Notes:** None



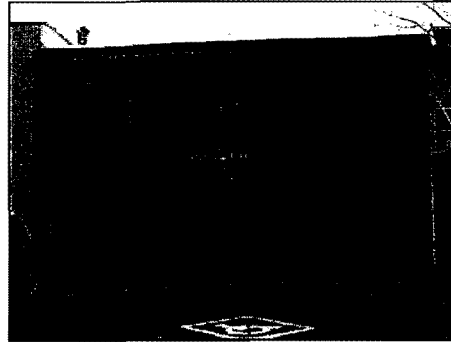
- **APN:**  
006-066-009-000
- **Address:**  
25 S. Main Street  
Colfax, CA 95713
- **Owner:**  
Edward F, Connie S.,  
Robert P., and Robert  
P. Marson

- **Building Name:** Marson's of Colfax
- **Current Use:** Marson's of Colfax Clothing/Shoe Store
- **Building Age:** Circa 1865
- **Architectural Style:** Early Western
- **Building SF:** 2,500
- **Improvements/Year Remodeled:** Unknown
- **# Stories:** 1
- **\*Structure Value:** \$49,144
- **Land SF:** .067 acre
- **\*Land Value:** \$10,341
- **Notable Property Features:** Building appears to be in good shape. Signage on side of building.
- **Vehicular Access:** Very good. Parking in front.
- **ADA Access:** Nearby
- **Opportunities:** Good window space for signs and view of merchandise.
- **Constraints:** Unknown
- **Notes:** Adjacent to open space/ park parcel.



- **APN:**  
006-066-010-000
- **Address:**  
27 S. Main Street  
Colfax, CA 95713
- **Owner:**  
Way West Properties

- **Building Name:** Unknown
- **Current Use:** Basement Wines
- **Building Age:** 1915
- **Architectural Style:** Early 20th Century Commercial
- **Building SF:** 1,850
- **Improvements/Year Remodeled:** 2000 most recent improvements.
- **# Stories:** 2
- **\*Structure Value:** \$179,948
- **Land SF:** .053 acre
- **\*Land Value:** \$33,738
- **Notable Property Features:** Great window space.
- **Vehicular Access:** Good. Parking in front.
- **ADA Access:** Yes. Ramp next door.
- **Opportunities:** Business draws customers to happy hour from 4-5 weekdays. Serves pizza, chili and sandwiches. Wine tasting and sales. Landscape improvements.
- **Constraints:** Unknown
- **Notes:** Lots of cigarette butts lying on the ground outside near bench/shrub/tree area.

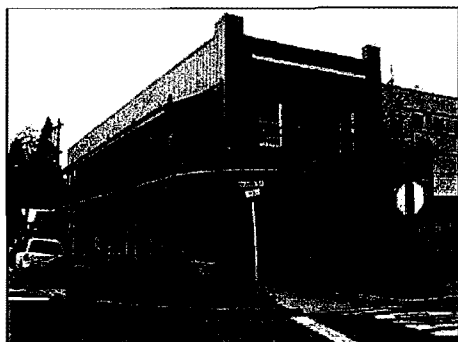


- **APN:**  
006-066-011-000
- **Address:**  
33 S. Main Street  
Colfax, CA 95713
- **Owner:**  
City of Colfax

- **Building Name:** Memorial Hall
- **Current Use:** City Hall
- **Building Age:** 1927
- **Architectural Style:** Early 20th Century Commercial
- **Building SF:** not available.
- **Improvements/Year Remodeled:** Currently remodeling/ updating.
- **# Stories:** 2
- **\*Structure Value:** Not available
- **Land SF:** .086 acre
- **\*Land Value:** Not available
- **Notable Property Features:** Two story, updated exterior & windows.
- **Vehicular Access:** Good
- **ADA Access:** Yes, w/Handicap parking in front.
- **Opportunities:** Central location, landscape improvements.
- **Constraints:** Unknown
- **Notes:** Bench outside is in poor condition. Access vents to building crawlspace are loose and need securing.

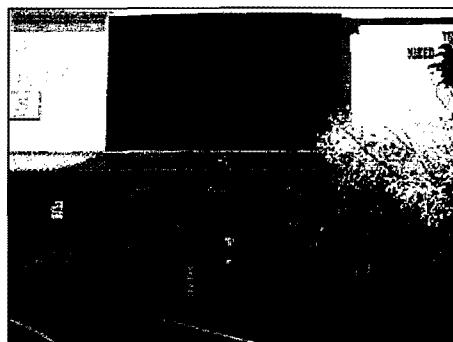
\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.





- **APN:**  
006-066-013-000
- **Address:**  
55 S. Main Street  
Colfax, CA 95713
- **Owner:**  
Ronnie W. &  
Constance K.  
Heilaman Trustees

- **Building Name:** Unknown
- **Current Use:** Blue Cat Studio, Connie's Decor, Heilaman Hall
- **Building Age:** Circa 1910
- **Architectural Style:** Eclectic
- **Building SF:** 4,000
- **Improvements/Year Remodeled:** Recent remodel in 2005.
- **# Stories:** 2
- **\*Structure Value:** \$112,740
- **Land SF:** .054 acre
- **\*Land Value:** \$29,722
- **Notable Property Features:** Connie's Decor is an Interior Design company with 5 suites/offices upstairs. Bldg is in good condition. Heilaman Hall is located on the side of the building.
- **Vehicular Access:** Good parking
- **ADA Access:** Nearby
- **Opportunities:** Corner property-exposure. Improve planting on corners.
- **Constraints:** Unknown
- **Notes:** Blue Cat Studio is resource for imaging, graphic design, marketing.



- **APN:**  
006-067-005-000
- **Address:**  
34 N. Main Street  
Colfax, CA 95713
- **Owner:**  
Gilbert A. Dalpino  
TTEE

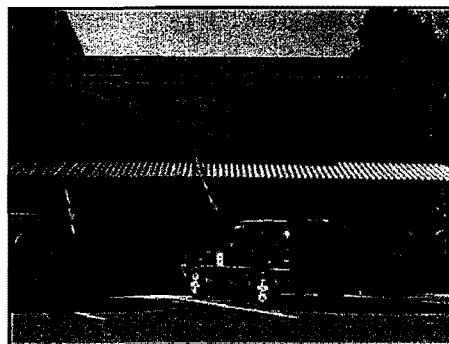
- **Building Name:** Railhead Saloon
- **Current Use:** Railhead Saloon
- **Building Age:** 1865
- **Architectural Style:** Early Western
- **Building SF:** 2,000
- **Improvements/Year Remodeled:** Unknown
- **# Stories:** 1
- **\*Structure Value:** \$26,487
- **Land SF:** .060 acre
- **\*Land Value:** \$8,402
- **Notable Property Features:** Bench area, planter.
- **Vehicular Access:** Acceptable
- **ADA Access:** Acceptable
- **Opportunities:** Central location, outdoor seating.
- **Constraints:** Unknown
- **Notes:** Well-used local establishment.

\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.



- **APN:**  
006-067-003-000
- **Address:**  
46 N. Main Street,  
Colfax, CA 95713
- **Owner:**  
Way West Properties

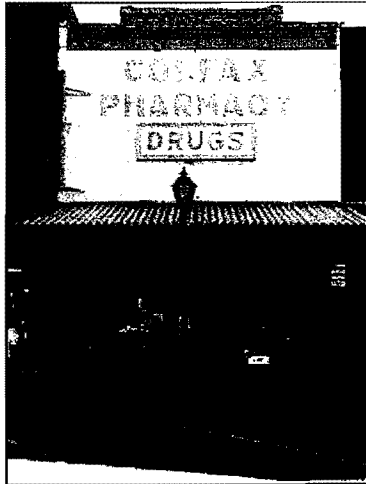
- **Building Name:** Jacob Keck
- **Current Use:** Main Street Pizza
- **Building Age:** 1865
- **Architectural Style:** Early Western
- **Building SF:** 4,522
- **Improvements/Year Remodeled:** 1980's (approx.)
- **# Stories:** 1
- **\*Structure Value:** \$125,962
- **Land SF:** .058 acre
- **\*Land Value:** \$50,609
- **Notable Property Features:** Screen door and angled inset store front with colored concrete paving; historic plaque, windows along full frontage, continuous smaller windows above, light fixture and hose bib in front, parapet and metal awning with wood posts.
- **Vehicular Access:** Acceptable
- **ADA Access:** Acceptable
- **Opportunities:** Set back space at entrance and wide sidewalks could accommodate small tables, add planting improvements to buffer sidewalk from parking.
- **Constraints:** Unknown.
- **Notes:** Concrete paving under overhang is in good shape.



- **APN:**  
006-067-002-000
- **Address:**  
54 N. Main Street,  
Colfax, CA 95713
- **Owner:**  
Wayne H. & Lou  
Anna Robinson

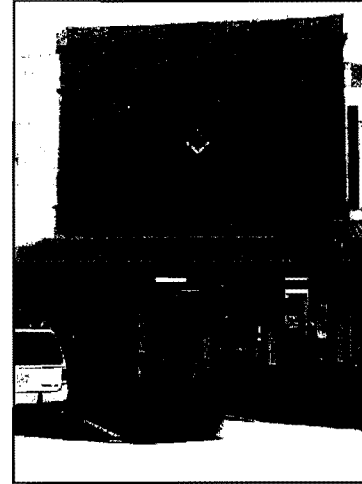
- **Building Name:** The Butcher Shoppe
- **Current Use:** 2 offices: North space vacant as of inventory date (January 2009) but currently occupied by the Quilt Shop (October, 2009). South space owner occupied as real estate business.
- **Building Age:** 1911
- **Architectural Style:** Early Western
- **Building SF:** 2,880
- **Improvements/Year Remodeled:** 1911 Daniel A. Russell and 1980's Moore & Robinson
- **# Stories:** 1
- **\*Structure Value:** \$25,113
- **Land SF:** .0101 acre
- **\*Land Value:** \$17,937
- **Notable Property Features:** Adjacent to Depot Street, historic plaque next to entrance, metal wall coverings, stone veneer and metal awning with wood posts.
- **Vehicular Access:** Acceptable
- **ADA Access:** Acceptable
- **Opportunities:** Large parapet signage space, add landscape improvements between sidewalk and parking.
- **Constraints:** Unknown.
- **Notes:** Both spaces share continuous parapet over entries and corrugated tin awning.

\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.



- **APN:**  
006-067-006-000
- **Address:**  
30 N. Main Street  
Colfax, CA 95713
- **Owner:**  
Thomas E. & Vicky  
L. Breslin
- **Building Name:** Colfax  
Drugstore
- **Current Use:** The Funny  
Pharm antique store

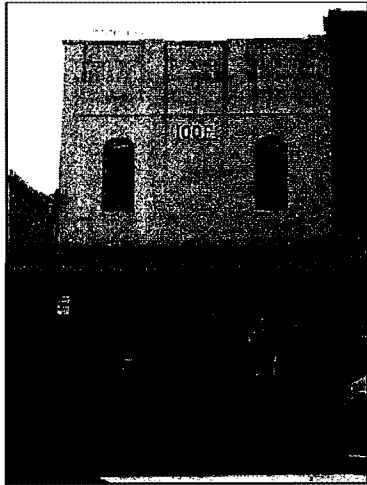
- **Building Age:** 1870
- **Architectural Style:** Early Western
- **Building SF:** 1,495
- **Improvements/Year Remodeled:** 1975
- **# Stories:** 1
- **\*Structure Value:** \$115,862
- **Land SF:** .053 acre
- **\*Land Value:** \$57,357
- **Notable Property Features:** Strong historical features, marble frontage pieces, detailed tile inset @ entry, rustic entry door w/screen window above, glass panels above.
- **Vehicular Access:** Acceptable
- **ADA Access:** Acceptable
- **Opportunities:** Lots of windows.
- **Constraints:** Unknown
- **Notes:** Nice ceiling inside, store closed during visit. Locally owned independent business.



- **APN:**  
006-067-007-000
- **Address:**  
24. N. Main Street  
Colfax, CA 95713
- **Owner:**  
Illinoistown Masonic  
Temple
- **Building Name:**  
Unknown
- **Current Use:** Antiques &  
Collectibles, Colfax  
Lodge (upstairs)

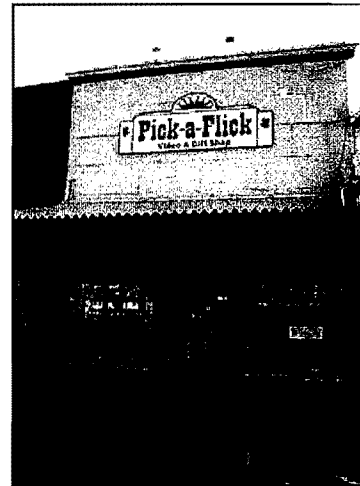
- **Building Age:** 1865
- **Architectural Style:** Early Western
- **Building SF:** 4,515
- **Improvements/Year Remodeled:** Unknown
- **# Stories:** 2
- **\*Structure Value:** \$26,331
- **Land SF:** .055 acre
- **\*Land Value:** \$8,558
- **Notable Property Features:** Inset, tall door/window openings, w/additional windows above brick facade on top below over hang. Wood paneling under windows.
- **Vehicular Access:** Acceptable
- **ADA Access:** Acceptable
- **Opportunities:** Landscape improvements, outdoor seating.
- **Constraints:** Double access doors.
- **Notes:** Colfax Lodge @ #14 side door w/hallway.

\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.



- **APN:**  
006-067-008-000
- **Address:**  
14 N. Main Street  
Colfax, CA 95713
- **Owner:**  
Odd Fellows #132 Int Ord
- **Building Name:** IOOF  
Hall
- **Current Use:** Printing  
Shop

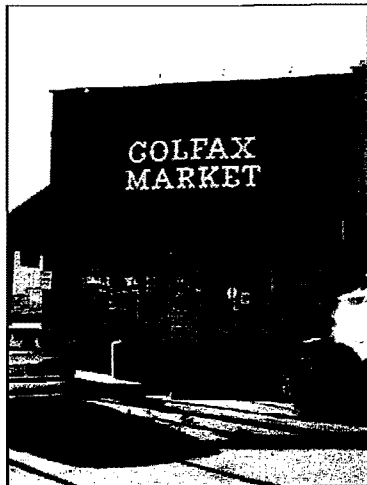
- **Building Age:** 1867
- **Architectural Style:** Early Western
- **Building SF:** 3,000
- **Improvements/Year Remodeled:** Overhang constructed approximately 30 years ago. Various interior updates.
- **# Stories:** 2
- **\*Structure Value:** \$15,862
- **Land SF:** .057 acre
- **\*Land Value:** \$7,925
- **Notable Property Features:** Wood paneling ceiling, large windows, oversized, double wood door, concrete entry, marble siding, AC unit above door.
- **Vehicular Access:** Acceptable
- **ADA Access:** Beginning of ADA ramp leading south.
- **Opportunities:** Main street.
- **Constraints:** Unknown.
- **Notes:** None



- **APN:**  
006-067-010-000
- **Address:**  
6 N. Main Street  
Colfax, CA 95713
- **Owner:**  
Robert K. Lobner (Life  
Est.) ET AL
- **Building Name:** Main  
Street Gift Company
- **Current Use:** Pick-a-Flick  
Video

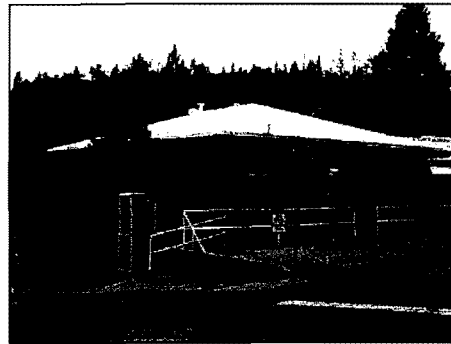
- **Building Age:** 1867
- **Architectural Style:** Early Western
- **Building SF:** 1,350
- **Improvements/Year Remodeled:** Framing above door/windows, fairly recent updated door/windows—windows above door replaced w/wood paneling—metal siding w/stone look.
- **# Stories:** 1
- **\*Structure Value:** \$15,734
- **Land SF:** .048 acre
- **\*Land Value:** \$8,047
- **Notable Property Features:** Highly used—popular. 2nd to corner property at intersection.
- **Vehicular Access:** Acceptable
- **ADA Access:** Acceptable
- **Opportunities:** Increase planting, maybe buffer/tree well.
- **Constraints:** Unknown
- **Notes:** Very popular store, central location.

\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.



- **APN:**  
006-067-011-000
- **Address:**  
2 N. Main Street  
Colfax, CA 95713
- **Owner:**  
Manjiv Raj ET AL
- **Building Name:** Colfax Market
- **Current Use:** Colfax Market/Liquor Store

- **Building Age:** 1865
- **Architectural Style:** Early Western
- **Building SF:** 3,375
- **Improvements/Year Remodeled:** Unknown, original brick on north side, stucco front update, updated windows/doors.
- **# Stories:** 1
- **\*Structure Value:** \$110,263
- **Land SF:** .065 acre
- **\*Land Value:** \$71,669
- **Notable Property Features:** Fallout shelter in basement. Concrete trash receptacle-non matching wood signage.
- **Vehicular Access:** Acceptable
- **ADA Access:** Acceptable
- **Opportunities:** Corner property-exposure. Improve planting on corners; paving in street/crosswalks.
- **Constraints:** Unknown.
- **Notes:** Anchor business with local patronage.



- **APN:**  
006-071-002-000
- **Address:**  
10 N. Auburn Street  
Colfax, CA 95713
- **Owner:**  
Colfax Telephone Exchange

- **Building Name:** Unknown
- **Current Use:** Verizon telephone exchange
- **Building Age:** Unknown—Updated approximately 40 yrs ago.
- **Architectural Style:** Modern
- **Building SF:** Not available
- **Improvements/Year Remodeled:** N/A
- **# Stories:** 1
- **\*Structure Value:** Not available
- **Land SF:** .119 acre
- **\*Land Value:** Not available
- **Notable Property Features:** Utility building with connected storage units in back.
- **Vehicular Access:** Acceptable—possible parking in adjacent lot in back.
- **ADA Access:** Unknown
- **Opportunities:** Buffering, improving views.
- **Constraints:** Pedestrian layout/flow undesirable.
- **Notes:** Corner of Grass Valley Street and Auburn Avenue.

\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.



- **APN:**  
006-071-003-000
- **Address:**  
33 E. Grass Valley  
Street  
Colfax, CA 95713
- **Owner:**  
John W. Demorest  
Trustee ET AL

- **Building Name:** Unknown
- **Current Use:** John Demorest General Dentistry
- **Building Age:** Unknown
- **Architectural Style:** Cape Cod
- **Building SF:** 1,796
- **Improvements/Year Remodeled:** Unknown
- **# Stories:** 1
- **\*Structure Value:** \$279,913
- **Land SF:** .172 acre
- **\*Land Value:** \$40,913
- **Notable Property Features:** Nice landscaping and inviting entry/appearance
- **Vehicular Access:** Good
- **ADA Access:** No
- **Opportunities:** Easy access with parking next door. Possible outdoor event location/gathering. Across from hotel.
- **Constraints:** Unknown
- **Notes:** None



- **APN:**  
006-071-005-000
- **Address:**  
Connected to Residence at 45 E. Grass  
Valley Street  
Colfax, CA 95713
- **Owner:**  
Paul M. Johnson

- **Building Name:** N/A
- **Current Use:** Open land next to RR tracks and appears to be used as drive way to residence for APN 006-071-006-000
- **Building Age:** N/A
- **Architectural Style:** N/A
- **Building SF:** N/A
- **Improvements/Year Remodeled:** N/A
- **# Stories:** N/A
- **\*Structure Value:** N/A
- **Land SF:** .031 acre
- **\*Land Value:** Not available
- **Notable Property Features:** Used as driveway and parking for residence.
- **Vehicular Access:** Unpaved access.
- **ADA Access:** N/A
- **Opportunities:** Landscaping/ pedestrian sidewalk improvements.
- **Constraints:** Adjacent to railroad.
- **Notes:** None

\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.



- **APN:**  
006-071-006-000
- **Address:**  
45 E. Grass Valley  
Street  
Colfax, CA 95713
- **Owner:**  
Paul M. Johnson

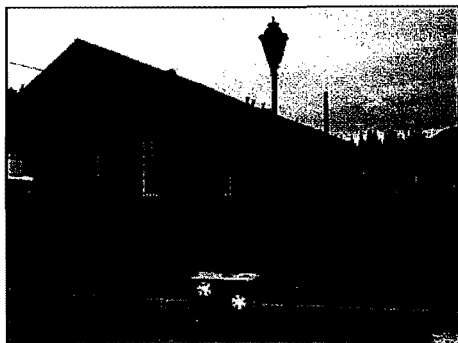
- **Building Name:** Unknown
- **Current Use:** Residence
- **Building Age:** 1958
- **Architectural Style:** Cape Cod
- **Building SF:** 956 (obviously larger)
- **Improvements/Year Remodeled:** Unknown
- **# Stories:** 2
- **\*Structure Value:** \$95,043
- **Land SF:** .056 acre
- **\*Land Value:** \$26,921
- **Notable Property Features:** Two story residence which appears to have been built between two parcels.
- **Vehicular Access:** Good
- **ADA Access:** Unknown
- **Opportunities:** Unknown
- **Constraints:** Unknown
- **Notes:** Backyard is very small and needs brush to be cleared.



- **APN:**  
006-071-007-000\*\*
- **Address:**  
2 S. Main Street  
Colfax, CA 95713
- **Owner:**  
Union Pacific Railroad  
Company

- **Building Name:** US Bank
- **Current Use:** US Bank
- **Building Age:** Circa 1970's
- **Architectural Style:** Modern with some Eclectic features
- **Building SF:** Not available
- **Improvements/Year Remodeled:** Not available
- **# Stories:** 1
- **\*Structure Value:**
- **Land SF:** 5.62 acres\*\*
- **\*Land Value:** Not available
- **Notable Property Features:** Lots of parking and trees/landscaping.
- **Vehicular Access:** Great
- **ADA Access:** Acceptable
- **Opportunities:** Corner location, improve landscaping.
- **Constraints:** Unknown
- **Notes:** Corner of S. Main Street and E. Grass Valley Street.
- **\*\***- this parcel is connected 1, 2, 3 & 4 N. Main Street which is all part of 5.62 acres. See page 10.

\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.



- **APN:**  
006-071-007-000\*\*
- **Address:**  
1, 2, 3 & 4 N. Main  
Street  
Colfax, CA 95713
- **Owner:**  
Union Pacific Railroad  
Company

- **Building Name:** The Depot
- **Current Use:** #1—Calamity Janes, #2—Tanya's Shear Full Service Salon, #3—?, #4—Colfax Antiques
- **Building Age:** Circa 1880's
- **Architectural Style:** Stickley/Eastlake Eclectic
- **Building SF:** Not available
- **Improvements/Year Remodeled:** Most recent renovations were done in 2005.
- **# Stories:** 1
- **\*Structure Value:** Not available
- **Land SF:** 5.62 acres\*\*
- **\*Land Value:** Not available
- **Notable Property Features:** Great parking in front of building and entire parking lot in back. Public restrooms have separate building with ADA ramp.
- **Vehicular Access:** Good
- **ADA Access:** No. Stairs at all entrances.
- **Opportunities:** Wood deck all the way around building. Great gathering space-safe from traffic.
- **Constraints:** Unknown
- **Notes:** \*\* - This parcel is connected to the US Bank parcel which is all part of 5.62 acres.



- **APN:**  
006-071-008-000
- **Address:**  
99 Railroad Avenue  
Colfax, CA 95713
- **Owner:**  
City of Colfax

- **Building Name:** Colfax Passenger Depot
- **Current Use:** Train Station
- **Building Age:** 1905
- **Architectural Style:** Colonial Revival
- **Building SF:** Not available
- **Improvements/Year Remodeled:** Most recent renovations were done in 2005.
- **# Stories:** 1
- **\*Structure Value:** Not available
- **Land SF:** 1.589 acre
- **\*Land Value:** Not available
- **Notable Property Features:**
- **Vehicular Access:** Good
- **ADA Access:** Good
- **Opportunities:** Improve pedestrian connection to the main street historic downtown. Railcar on property could be functional space; enhances railroad theme.
- **Constraints:** Unknown
- **Notes:** Recently restored, the Passenger Depot is home to the Colfax Heritage Museum, the Colfax Area Chamber of Commerce and the Visitor Center.



\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.





- **APN:**  
006-071-009-000
- **Address:**  
Along E. Grass Valley  
Street  
Colfax, CA 95713
- **Owner:**  
Unknown

- **Building Name:** N/A
- **Current Use:** Parking lot next to John Demorest General Dentistry
- **Building Age:** N/A
- **Architectural Style:** N/A
- **Building SF:** N/A
- **Improvements/Year Remodeled:** Unknown
- **# Stories:** N/A
- **\*Structure Value:** N/A
- **Land SF:** .056 acre
- **\*Land Value:** \$26,921
- **Notable Property Features:** Parking lot.
- **Vehicular Access:** Acceptable
- **ADA Access:** No
- **Opportunities:** Possible outdoor event location/ gathering. Next to dentist office and across from hotel.
- **Constraints:** Unknown
- **Notes:** None



- **APN:**  
006-071-010-000
- **Address:**  
Along E. Grass Valley  
Street and N Main  
Colfax, CA 95713
- **Owner:**  
City of Colfax

- **Building Name:** N/A
- **Current Use:** Roy Toms Plaza
- **Building Age:** Not available
- **Architectural Style:** Victorian/Eclectic
- **Building SF:** N/A
- **Improvements/Year Remodeled:** Unknown
- **# Stories:** N/A
- **\*Structure Value:** Unknown
- **Land SF:** .502 acre
- **\*Land Value:** Unknown
- **Notable Property Features:** Outdoor seating/gazebo.
- **Vehicular Access:** Good
- **ADA Access:** Yes
- **Opportunities:** Pedestrian plaza, adjacent to event/ farmers market location. Railcar contributes to railroad theme.
- **Constraints:** Unknown
- **Notes:** None

\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.



- **APN:**  
006-072-002-000
- **Address:**  
15 S. Auburn Street  
Colfax, CA 95713
- **Owner:**  
Unknown

- **Building Name:** N/A
- **Current Use:** Residence
- **Building Age:** 1927
- **Architectural Style:** Vernacular
- **Building SF:** 1,008
- **Improvements/Year Remodeled:** Unknown
- **# Stories:** Unknown
- **\*Structure Value:** \$187,000
- **Land SF:** .179 acre
- **\*Land Value:** Not available
- **Notable Property Features:** Unknown
- **Vehicular Access:** Unknown
- **ADA Access:** Unknown
- **Opportunities:** Unknown
- **Constraints:** Unknown
- **Notes:** Residence located behind Colfax Hotel. Photo, value and building age obtained from Zillow.com.

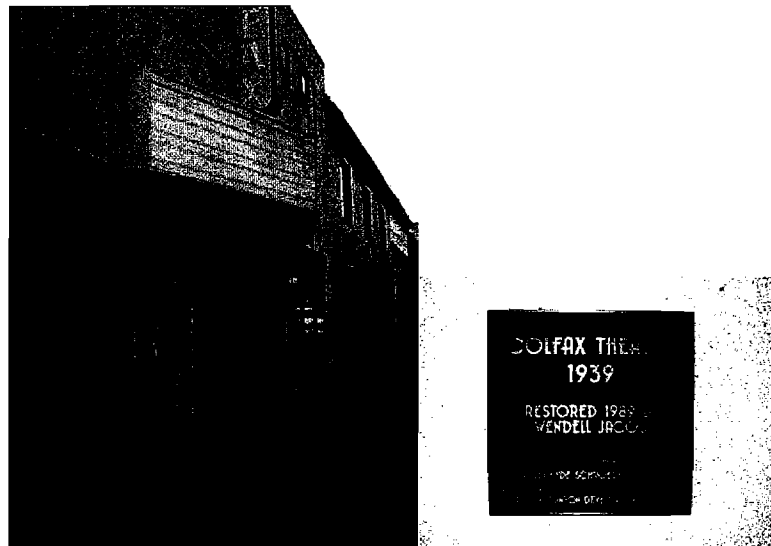
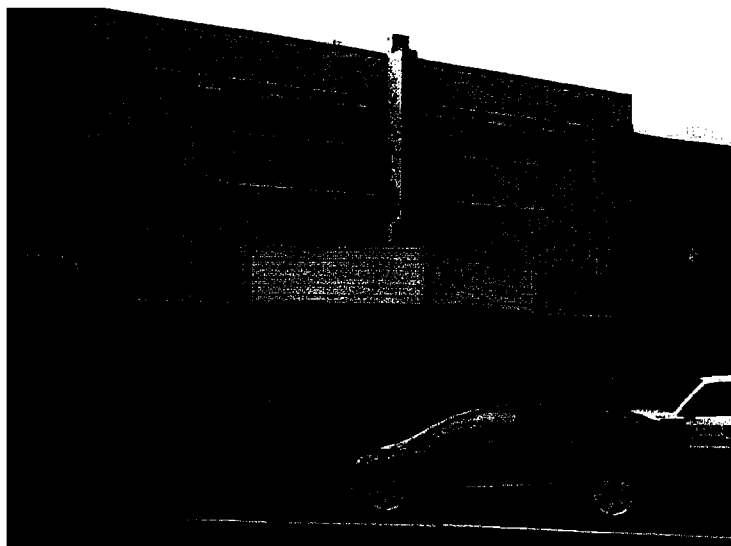


- **APN:**  
006-091-006-000
- **Address:**  
2 Church Street  
Colfax, CA 95713
- **Owner:**  
County of Placer

- **Building Name:** Colfax Library
- **Current Use:** Colfax Library
- **Building Age:** Circa 1890's
- **Architectural Style:** Mission Revival
- **Building SF:** 3,136
- **Improvements/Year Remodeled:** Renovation of the library is being planned.
- **# Stories:** 1
- **\*Structure Value:** Not available
- **Land SF:** .162 acre
- **\*Land Value:** Not available
- **Notable Property Features:** Ramp at crosswalk and along front of building.
- **Vehicular Access:** Good. Parking in back and on side.
- **ADA Access:** OK
- **Opportunities:** Across from pocket park.
- **Constraints:** No sidewalk on east side.
- **Notes:** Located on south corner of Main and Church Street.

## **Appendix B: Historic Core Property Inventory – Vacant**

---



- **APN:** 006-066-012-000
- **Address:** 49 S. Main Street, Colfax, CA 95713
- **Owner:** Wendell P. Jacob Trustee
- **Building Name:** Colfax Theatre
- **Current Use:** Vacant
- **Building Age:** 1939
- **Architectural Style:** Art Deco
- **Building SF:** 3,700
- **Improvements/Year Remodeled:** Restored in 1989 by Wendell Jacob. Interior updated from movie theatre to theatre/ music venue.
- **# Stories:** 2
- **\*Structure Value:** \$309,288
- **Land SF:** .085 acre
- **\*Land Value:** \$50,608
- **Notable Property Features:** Decorative architectural

façade with original neon theatre signage, program display cases, and double entrance doors, diagonal parking configuration.

- **Vehicular Access:** Good
- **ADA Access:** Yes, two ADA ramps to the north.
- **Opportunities:** Provide evening destination with live music, theatre, community events; building character draws attention.
- **Constraints:** Occupancy limitations.
- **Notes:** Property has been vacant for some time, and has been remodeled to hold theatre and music events. Interior elements such as heating and seating is said to need updating.

\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.



- **APN:** 006-066-021-000
- **Address:** 1 S. Main Street, Colfax, CA 95713
- **Owner:** Donald M. Frisk Trustees
- **Building Name:** Xmex
- **Current Use:** Xmex restaurant (closed), Retail space (vacant), Hair Salon, Art Gallery (opening soon), 6 offices on second floor (vacant)
- **Building Age:** 1972
- **Architectural Style:** Contemporary interpretation of Early Western
- **Building SF:** 7,392
- **Improvements/Year Remodeled:** 1980's (approx.)
- **# Stories:** 2
- **\*Structure Value:** \$287,315
- **Land SF:** .169 acre
- **\*Land Value:** \$87,429
- **Notable Property Features:** Prime location at corner of

E. Grass Valley Road and South Main Street. Some improvements recommended to air conditioner and shed below unit on the backside of building. Wood siding and brick appears in very good condition, building has lots of appeal.

- **Vehicular Access:** Great—High traffic corner in pedestrian zone, parking in front and side of building.
- **ADA Access:** Good
- **Opportunities:** Landscape and crosswalk improvements, mixed use lease and live/ work spaces.
- **Constraints:** Difficult to create separate recognizable identities for tenant businesses from street view; Signs of vacated businesses should be removed.
- **Notes:** Building has ground floor space for lease; there are 6 office spaces upstairs that are also available.

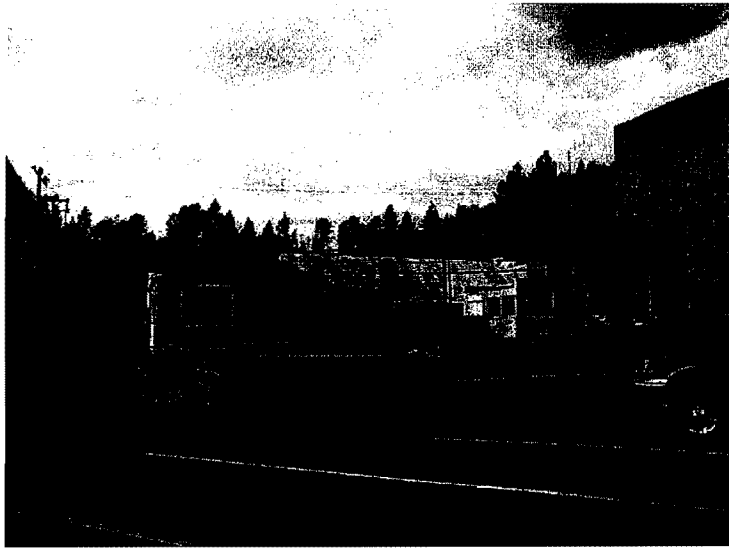
\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.



- **APN:** 006-066-027-000
- **Address:** Street number unknown; S. Main Street, Colfax, CA 95713
- **Owner:** Edward F., Connie S., Robert P., Robert P. Marson
- **Building Name:** N/A
- **Current Use:** Vacant lot—left 1/3 side of picture (lot connected to APN 006-066-028-000)
- **Building Age:** N/A
- **Architectural Style:** N/A
- **Building SF:** N/A
- **Improvements/Year Remodeled:** N/A
- **# Stories:** N/A
- **\*Structure Value:** N/A
- **Land SF:** .057 acre
- **\*Land Value:** \$12,307

- **Notable Property Features:** Open space area/ existing concrete walks throughout parcel, turf and cobble. Metal fencing adjacent to sidewalk.
- **Vehicular Access:** Diagonal parking in front of property.
- **ADA Access:** Good
- **Opportunities:** Good location. Potential development could include multi-story mixed use development with live/ work environments above and businesses below, including furniture sales and fabrication, computer peripheral sales and repair. Another possibility is to develop a small park with playground and mini outdoor theatre/ band stand.
- **Constraints:** Small size. Historically counted as park acreage for parks planning purposes by City of Colfax. Potential construction limitations due to impacts on adjacent structures
- **Notes:** Build to suit or improve features as public open space in prime Main Street location.

\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.

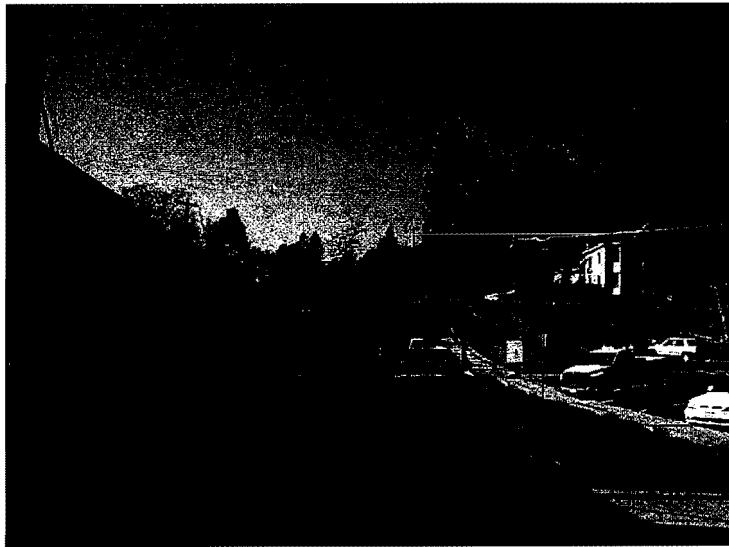


- **APN:** 006-066-028-000
- **Address:** Street number unknown; S. Main Street, Colfax, CA 95713
- **Owner:** Scott Miles
- **Building Name:** N/A
- **Current Use:** Vacant lot—right 2/3 side of picture (lot connected to APN 006-066-027-000)
- **Building Age:** N/A
- **Architectural Style:** N/A
- **Building SF:** N/A
- **Improvements/Year Remodeled:** N/A
- **# Stories:** N/A
- **\*Structure Value:** N/A
- **Land SF:** .116 acre
- **\*Land Value:** \$52,653
- **Notable Property Features:** Open space area/ existing

concrete walks throughout parcel, turf and cobble. Metal fencing adjacent to sidewalk.

- **Vehicular Access:** Diagonal parking in front of property.
- **ADA Access:** Good
- **Opportunities:** Good location. Potential development could include multi-story mixed use development with live/ work environments above and businesses below, including furniture sales and fabrication, computer peripheral sales and repair. Another possibility is to develop a small park with playground and mini outdoor theatre/ band stand.
- **Constraints:** Small size. Historically counted as park acreage for parks planning purposes by City of Colfax. Potential construction limitations due to impacts on adjacent structures
- **Notes:** Build to suit or improve features as public open space in prime Main Street location.

\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.



- **APN:** 006-067-001-000
- **Address:** 58 N. Main Street, Colfax, CA 95713
- **Owner:** Christopher M. & Brenda L. Toepfer TTEE
- **Building Name:** N/A
- **Current Use:** Vacant lot
- **Building Age:** N/A
- **Architectural Style:** N/A
- **Building SF:** N/A
- **Improvements/Year Remodeled:** N/A
- **# Stories:** N/A
- **\*Structure Value:** N/A
- **Land SF:** .067 acre
- **\*Land Value:** \$90,000

- **Notable Property Features:** Corner lot, good exposure to Main Street and Depot Street, existing sidewalk, alley behind property parking adjacent to Depot Street and along Main Street, existing concrete curb at frontage.
- **Vehicular Access:** Good
- **ADA Access:** Improve cross walk at Depot Street.
- **Opportunities:** Potential development could include single-story space for example food service, or business offering furniture sales and fabrication, computer peripheral sales and repair, etc.
- **Constraints:** Small lot size; potential construction limitations due to impacts on adjacent structures
- **Notes:** Build to suit prime location on street corner near historic depot building.

\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.



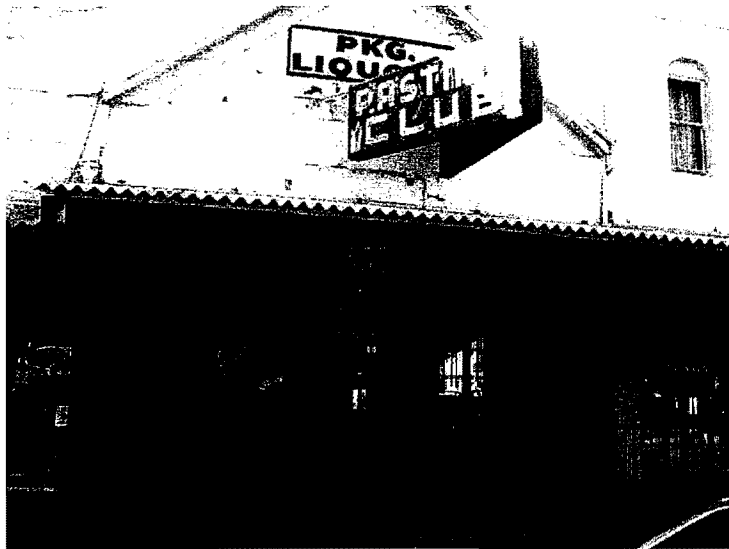


- **APN:** 006-067-004-000
- **Address:** 42 N. Main Street, Colfax, CA 95713
- **Owner:** Christopher Liu
- **Building Name:** Jacob Keck
- **Current Use:** Vacant—previously a restaurant
- **Building Age:** 1865
- **Architectural Style:** Early Western
- **Building SF:** 1,958
- **Improvements/Year Remodeled:** 1970's (approx.)
- **# Stories:** 1
- **\*Structure Value:** \$117,565
- **Land SF:** .053 acre
- **\*Land Value:** \$111,322
- **Notable Property Features:** Historic character, original brick exposed, painted brick, doors and entries have original wood shutters with updated doors and windows which are setback into the building, painted rooster sign-

age on building frontage above awning, bench, planting pots and landscape planter with tree in front.

- **Vehicular Access:** OK
- **ADA Access:** OK
- **Opportunities:** Space is suited for food service-type establishment with great historic small-town character, landscape improvements in front planter—possibly expand planter. Covered patio in rear of building could be used for dining.
- **Constraints:** Abandoned signage for closed business; limited window frontage.
- **Notes:** Restaurant location, business for sale. Metal continuous awning above supported with wood posts, parapet façade.

\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.

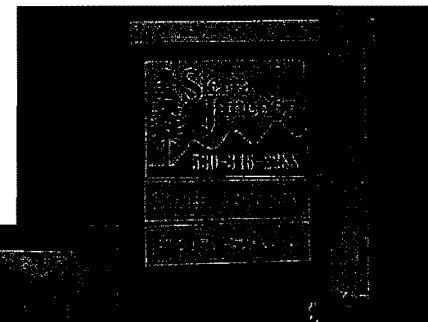
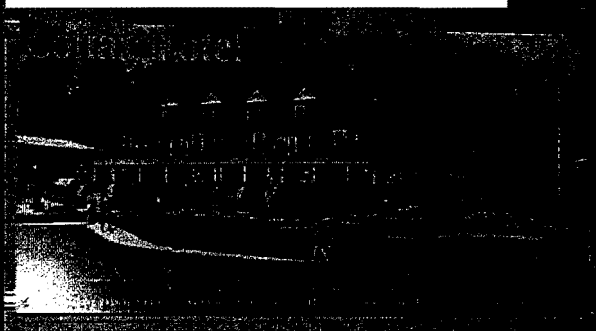


- **APN:** 006-067-009-000
- **Address:** 10 N. Main Street, Colfax, CA 95713
- **Owner:** Elvira Bianchi Trustee
- **Building Name:** Murphy's Saloon
- **Current Use:** Vacant—Pastime Club/ Bar
- **Building Age:** 1866
- **Architectural Style:** Early Western
- **Building SF:** 3,000
- **Improvements/Year Remodeled:** 1975
- **# Stories:** 1
- **\*Structure Value:** \$12,688
- **Land SF:** .056 acre
- **\*Land Value:** \$7,925
- **Notable Property Features:** Old wheel rims worked into architecture; decorated door posts and metal awning overhang has metal posts with curved metal ornament-

tals. Existing signage painted on frontage wall and a larger triangular sign is mounted out above the awning.

- **Vehicular Access:** Good
- **ADA Access:** ADA ramp and railings in front of overhang walk.
- **Opportunities:** Space is suited for food service-type bistro with great historic small-town character, landscape improvements in front planter—possibly expand planter.
- **Constraints:** Historic sign adds character but may conflict with future business use.
- **Notes:** Walk under overhang has steep slope, ramp in place-looks adequate. Interesting signage & minor related door decoration (gold pan, shovel, pick), continuous metal awning steps down 12" to the south side neighboring building.

\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.



- **APN:** 006-072-001-000
- **Address:** 10 Grass Valley Street, Colfax, CA 95713
- **Owner:** James F. Payne
- **Building Name:** Colfax Hotel; "Gillen" Hotel
- **Current Use:** Vacant
- **Building Age:** 1903
- **Architectural Style:** Colonial Revival with some Queen Anne style elements
- **Building SF:** 11,952
- **Improvements/Year Remodeled:** Unknown
- **# Stories:** 3
- **\*Structure Value:** \$617,478
- **Land SF:** .316 acre
- **\*Land Value:** \$154,367

- **Notable Property Features:** Free-standing historic building, 16 rooms, high ceilings, parking lot and extra parking in front, approved plans and zoned CR-H with approval for hotel, very visible location, building anchors corner, and is in walking distance from station, downtown.
- **Vehicular Access:** Good, frontage along Railroad Street only accessible from the south side.
- **ADA Access:** Good, back of building.
- **Opportunities:** Proximity to I-80 and railroad station, an immediate need for a hotel, facility will function as a draw for the Historic Downtown.
- **Constraints:** Substantial cost to complete renovations related to building age, historic preservation.
- **Notes:** None

\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.



- **APN:** 006-091-007-000
- **Address:** 121 S. Main Street, Colfax, CA 95713
- **Owner:** Auburn Creeks Real Estate Inc.
- **Building Name:** N/A
- **Current Use:** Residence (vacant—being remodeled)
- **Building Age:** 1876
- **Architectural Style:** Victorian/Eclectic
- **Building SF:** 1,996
- **Improvements/Year Remodeled:** 2008/2009
- **# Stories:** 2
- **\*Structure Value:** \$135,834
- **Land SF:** .148 acre
- **\*Land Value:** \$118,854

- **Notable Property Features:** Newly remodeled in new condition, wrap around porches and new roof windows and doors and siding
- **Vehicular Access:** Acceptable
- **ADA Access:** No, stairs only at front entry.
- **Opportunities:** Opportunity for smaller shop on ground floor with rental for upper story, porches integrated into residential for live/ work uses. Possible Bed & Breakfast.
- **Constraints:** Converted to residence. Not sure of status (i.e. to be sold or occupied by owner).
- **Notes:** None

\*Land and structure values obtained from Placer County Tax Assessor's office—January 2008 data.

# **Part C: City of Colfax Historic Downtown Conceptual Master Plan**

**City of Colfax**

# **Historic District Conceptual Master Plan**

October 2009

Prepared for:  
City of Colfax



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ENVIRONMENTAL CONSULTING • PLANNING • LANDSCAPE ARCHITECTURE

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# 1. INTRODUCTION

The **Colfax Historic Downtown Conceptual Master Plan** consists of three individual graphic plans that together provide a coordinated approach to future improvements efforts within the Historic District and Core of the City. The individual plans can be summarized as follows.

The **Gateways and Connection Plan** provides an overall concept of connectivity in the District and to the surrounding areas. The plan suggests gateway locations at strategic points that will provide a sense of arrival and celebrate the unique historic character of the area. The plan shows potential pedestrian, trail, and bicycle routes throughout the Historic District and beyond, which increases connectivity and invites the public to explore and venture throughout the Historic District, and into the Historic Core. The plan further identifies potential open space and civic use areas locations, additional parking opportunities, new connections, service nodes and streetscape improvements.

The **Colfax Historic Core Conceptual Master Plan** features specific improvements within the Historic Core of the City, such as street improvements and intersections, decorative paving areas, landscape upgrades, and decorative crosswalks connecting a continuous pattern of pedestrian-scale streets and corridors. These features will support the Historic Core in becoming an economically vital and socially vibrant center within the City. The plan incorporates existing features that already help to sustain pedestrian circulation such as the existing park by the library, the historic caboose, and the Freight Depot.

The **Phasing Plan** for the **Historic Core Conceptual Master Plan** provides a recommended phased implementation approach for the various improvements to the Historic Core. There are five suggested phases for the improvements that can be developed as funding becomes available. Priority has been given to those improvements which are likely to provide the greatest immediate benefit.

- **Phase 1** includes the area along Grass Valley Street immediately before the turn at the Colfax Hotel through the Main Street intersection. This phase includes a landscaped center median on Grass Valley Street, connecting crosswalks with decorative paving, landscape and sidewalk improvements. Cost: \$759,436
- **Phase 2** includes the North Main street section from the Grass Valley intersection to the north edge of the Historic Core. Improvements include a roundabout at Depot Street and Main Street, crosswalks with decorative paving, parking improvements and landscape enhancements, a mini plaza west of the Depot, and a potential sidewalk widening on the west side of Main Street. Cost: \$406,616
- **Phase 3** is located along South Main Street to Church Street and includes parking improvements and landscape enhancements along the street, and additional parking south of the existing bank parking, with a pedestrian connection on the east side of the street. The Church Street intersection has connecting crosswalks with decorative paving, landscape and sidewalk improvements. Cost: \$182,653



- **Phase 4** addresses the area east of the historic Freight Depot building with new parking configuration and circulation, and an urban plaza around the existing gazebo and caboose. The urban plaza will connect to a new train drop-off that will service the westbound train track. This phase also reserves an area for a future civic use area and additional parking. Cost: \$1,029,188
- **Phase 5** includes the existing pocket park adjacent to the library, where the plan provides for decorative crosswalks, interior park improvements around the existing elements, and a landscape upgrade. In addition, the plan suggests some additional parking with a pedestrian connection to the east side of South Main Street. Cost: \$186,738

## 2. FUNDING FOR THE COLFAX HISTORIC DISTRICT MASTER PLAN

The funding needed to implement streetscape improvements to the City of Colfax's Historic Core, as well as other elements of the Historic District Master Plan will come from a variety of sources. These may include grants from public or private agencies, general fund monies, revenue bonds, and donations.

### 2.1 Grants

**Save America's Treasures** is a national public-private partnership established in 1998 to recognize and protect historic sites and artifacts. Grants from this program are administered by the National Park Service and are available for the preservation or conservation of nationally significant historic structures or sites, including both historic districts and buildings. Grants are provided as dollar-for-dollar matching grants ranging from \$125,000 to \$700,000.

Matching grants are available from **Preserve America**, a Federal initiative, to support community efforts to develop sustainable uses of their historic sites, with an emphasis on educational and economic opportunities related to heritage tourism. Cities must be recognized as a "Preserve America Community" to be eligible to apply for a Preserve America grant. Grant amounts range from \$20,000 to \$250,000.

A number of Federal agencies administer grant programs for transportation or community development. **Community Development Block Grants** are available through the U.S. Department of Housing and Urban Development. They can be used for a variety of purposes, but a minimum of 70% of the funds received must be used to benefit low- and moderate-income persons. The **Federal Highway Administration** offers transportation enhancement grants for projects that improve the cultural, historic, aesthetic, and environmental aspects of transportation infrastructure.

The **National Trust Preservation Fund** provides matching grants ranging from \$500 to \$5,000 dollars for preservation planning and educational efforts. Grants ranging from \$2,500 to \$10,000 are available for projects that preserve or recapture an authentic sense of place from the **Johanna Favrot Fund for Historic Preservation**. Applications for these grants are made through the private, non-profit organization National Trust for Historic Preservation, whose western regional office is located in San Francisco.

**Reconnecting America** and the **Center for Transit-Oriented Development** are non-profit organizations that work to promote the preservation and expansion of railways and transit-oriented development throughout the United States. Grants may be available to expand the train station and improve its connection to the rest of the historic district.

The **Sierra Nevada Conservancy**, a state agency, provides both technical and economic assistance for projects to improve the environmental, social, and economic well-being of communities in the Sierra Nevada. Revitalization of the Historic Downtown would meet a number of the

Conservancy's program objectives, such as providing increased opportunity for tourism and recreation; to protect, conserve and restore historical and cultural resources; and to assist the regional economy. The Sierra Nevada Conservancy administers grants funded primarily by bond funds. Currently, grant awards are frozen due to California's budget situation, but additional funding is anticipated to be available in the future.

Some funding is available specifically for the proposed museum and educational centers. **The National Railway Historical Society**, a private non-profit organization, annually awards matching grants for projects that preserve, research, educate, or publish railway history. Five thousand dollars is the maximum award per year per project. The North American Railway Foundation is a non-profit private operating foundation that seeks to preserve the history of railroads and support railroad safety, efficiency, and technology. Instead of providing grant funds, the NARF takes an active and direct role in projects and typically works with non-profit organizations and rail labor organizations. NARF also works with local governments to improve safety at railroad grade crossings. Funding for museums is available through the **National Endowment for the Arts** and the **Institute of Museum and Library Science**.

## **2.2 Local Funding**

Funding for improvements to the Historic Core can be generated locally in three ways. Money can be allocated for projects from the City's General Fund. The City can work to pass a revenue bond, which would be repaid by an increase in property or sales taxes over a designated period. This would require approval by voters. Finally, the City could establish a special district that would collect fees from property owners within the district. This would also require approval from voters in the proposed district.

## **2.3 Other Funding Sources**

If the Historic District was designated as a redevelopment project area by the Placer County Redevelopment Agency, then monies would be available through tax increment funds. Special Federal and State Grants are available for work in adopted redevelopment project areas.

Transferring overhead electrical facilities to underground trenches can be paid for through **PG&E's Rule 20A** program. This requires coordination with PG&E and other local utility providers to ensure that all services attached to utility poles are modified to allow pole removal.

A **public-private partnership** is one option for developing the Railroad Museum/Transportation Center. This would allow more time to be dedicated to fundraising campaigns than might be possible by City staff. The museum would also be supported by visitor fees and donations once established.

**Endowments** and **sponsorships** from local businesses and residents are other funding options that are well-suited to a local museum. However, some streetscape improvements such as benches, picnic areas, trees, and park improvements are also strong candidates for individual or corporate sponsorship. Sponsorship offers an opportunity for increasing community involvement and interest in the project. The City should prepare a list of sponsorship opportunities and levels for each phase of the project.

## 2.4 Web Resources for More Information

National Trust for Historic Preservation <http://www.preservationnation.org/resources/find-funding/nonprofit-public-funding.html>

California Office of Historic Preservation <http://ohp.parks.ca.gov>

Save America's Treasures <http://www.saveameericastreasures.org>

Reconnecting America <http://www.reconnectingamerica.org>

Preserve America <http://www.preserveamerica.gov/overview.html>

<http://www.nps.gov/history/hps/hpg/PreserveAmerica/>

Summary of Federal grants [http://www.nps.gov/history/hps/hpg/downloads/Show\\_Me\\_the\\_Money2009.pdf](http://www.nps.gov/history/hps/hpg/downloads/Show_Me_the_Money2009.pdf)

Placer County Redevelopment Agency <http://www.placer.ca.gov/Departments/CEO/Redevelopment.aspx>

National Railroad Historical Society <http://www.nrhs.com/grant/index.html>

North American Railway Foundation <http://www.narfoundation.org>

Sierra Nevada Conservancy <http://www.sierranevadaconservancy.ca.gov/grants.html>

### **3. COST ESTIMATES BY PHASE**

**Colfax Historic Downtown Master Plan**  
**Preliminary Statement of Probable Improvement Cost**  
**PHASE 1 - 5**

10/29/2009

Phase		DESCRIPTION	Page	Total Phase
Phase 1		S. Auburn Street Corner, Grass Valley Street Narrowing, Main Street Intersection	1	\$759,436
Phase 2		N. Main Street, Including Street Section Past Depot Street, South of Evangeline's	2	\$406,616
Phase 3		S. Main Street, From Grass Valley Street Intersection Through W. Church Intersection.	3	\$182,653
Phase 4		Depot Plaza and Parking Lot, Civic Use Area	4	\$1,029,188
Phase 5		Pocket Park and Parking South of Park	5	\$186,738
			<b>Grand Total</b>	<b>\$2,564,632</b>

**Colfax Historic Downtown Master Plan**  
**Preliminary Statement of Probable Improvement Cost PHASE 1**  
**Grass Valley Street**  
10/29/2009

[Page 1]

CATEGORY	ITEM	DESCRIPTION	UNIT	QUANT	UNIT PRICE	AMOUNT
Site Preparation	1	Site Layout and Survey (Grass Valley Street Narrowing)	LS	1	\$10,000.00	\$10,000
	2	Construction Staging & Mobilization	LS	1	\$35,000.00	\$35,000
	3	Concrete Cutting, Demolition & Removal	SF	10000	\$4.50	\$45,000
	5	Asphalt Concrete Grinding (2") S. Auburn Street Corner)	SF	5000	\$1.50	\$7,500
	6	Roadway Excavation and Grading (Grass Valley Street)	LS	1	\$36,000.00	\$36,000
	7	Temporary Fencing/ Including Tree Protection	LS	1	\$4,000.00	\$4,000
	8	Erosion Control	LS	1	\$600.00	\$600
	9	Traffic Control	LS	1	\$2,000.00	\$2,000
Infrastructure System	1	Adjust Existing Drainage Structures to Grass Valley Street Narrowing	LS	1	\$10,000.00	\$10,000
	2	Electricity to Median (Including Sleeving)	LS	1	\$15,000.00	\$15,000
	3	Water Connection to Median (Including Sleeving)	LS	1	\$8,000.00	\$8,000
Site Work	1	Standard Concrete Paving (Grass Valley St. sidewalk)	SF	18000	\$5.00	\$90,000
	2	Enhanced Paving (Colored - Intersection Main St. / Grass Valley St.)	SF	3517	\$12.50	\$43,963
	3	Enhanced Paving (Colored Crosswalks)	SF	1775	\$12.50	\$22,188
	4	Asphalt Concrete Paving/ Including Class 2 Ag. Base (Grass Valley Street)	SF	5567	\$4.85	\$27,000
	5	Street Striping (Grass Valley Street)	LF	1000	\$9.50	\$9,500
	6	Curb and Gutter (Grass Valley Street)	LF	900	\$12.00	\$10,800
Amenities and Site Furnishings	1	Relocate Historic Street Light Fixtures (Grass Valley St. Narrowing)	LS	1	\$15,000.00	\$15,000
	2	Add 4 Historic Light Fixtures along Grass Valley Street	LS	4	\$5,000.00	\$5,000
	3	Directional Signage (Grass Valley St./ S. Auburn St.)	EA	3	\$2,000.00	\$6,000
	4	Entry Monument/ Gateway S. Auburn Street/ Oak	EA	1	\$120,000.00	\$120,000
Irrigation System	1	Irrigation System (new planting areas)	SF	2030	\$1.65	\$5,000
	2	Existing Irrigation System Upgrades & Repair	LS	1	\$1,800.00	\$1,800
	3	Smart Controller and Assembly/ Pedestal	LS	1	\$9,000.00	\$9,000
	4	Flow Sensor & Master Valve	LS	1	\$2,400.00	\$2,400
	5	Backflow Preventor and Enclosure	LS	1	\$6,000.00	\$6,000
Landscaping	1	Finish Grading	SF	3030	\$0.05	\$152
	2	Soil Amendment	SF	3030	\$0.20	\$606
	3	Shrub/ Groundcover Planting	SF	3030	\$2.75	\$8,333
	4	15 gallon Tree	EA	7	\$150.00	\$1,050
	5	24" Box Trees	EA	2	\$300.00	\$600
	6	Bark Mulch 3" Depth	CY	5	\$40.00	\$200
	7	Tree Root Barrier	LF	194	\$18.00	\$3,492
	8	Landscape Improvements at Station Along Grass Valley St.	LS	1	\$10,000.00	\$10,000
	9	90-Day Maintenance	LS	1	\$13,000.00	\$13,000
					<b>SUBTOTAL</b>	<b>\$584,181</b>
					15% PS & E	\$87,627
					15% Contingency	\$87,627
					<b>PHASE I TOTAL</b>	<b>\$759,436</b>

**Colfax Historic Downtown Master Plan**  
**Preliminary Statement of Probable Improvement Cost PHASE 2**  
**North Main Street**  
10/29/2009

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CATEGORY	ITEM	DESCRIPTION	UNIT	QUANT	UNIT PRICE	AMOUNT
Site Preparation	1	Site Layout and Survey	LS	1	\$15,000.00	\$15,000
	2	Construction Staging & Mobilization	LS	1	\$25,000.00	\$25,000
	3	Erosion Control	LS	1	\$3,600.00	\$3,600
	4	Temporary Fencing/ Including Tree Protection	LS	1	\$2,000.00	\$2,000
	5	Asphalt Concrete Grinding (2")	SF	31328	\$1.50	\$46,992
	6	Traffic Control	LF	2500	\$4.00	\$10,000
Infrastructure System	1	Adjust/ Abandon Existing Drainage Structures	LS	1	\$10,000.00	\$10,000
	2	Sleeving 4" to Roundabout	LS	1	\$2,000.00	\$2,000
Site Work	1	Standard Concrete Paving	SF	4250	\$5.00	\$21,250
	2	Enhanced Paving Crosswalks & Roundabout (Colored)	SF	1025	\$12.50	\$12,813
	3	Asphalt Paving/ Including Class 2 Ag. Base	SF	1034	\$4.85	\$5,000
	4	Concrete Steps & Ramp at Evangeline's	LS	1	\$5,500.00	\$5,500
	5	Concrete Parking Stop Bar	EA	37	\$185.00	\$6,845
	6	Striping	LF	600	\$9.50	\$5,700
	7	Curb and Gutter	LF	775	\$12.00	\$9,300
Amenities and Site Furnishings	1	Relocate/ Adjust Street Lighting	LS	1	\$15,000.00	\$15,000
	2	Replace existing wood light pole w/ historic light fixture (at Main St.)	LS	1	\$2,000.00	\$2,000
	3	Relocate Flag Pole/ New Footing	LS	1	\$4,500.00	\$4,500
	4	Remove Existing Trash Cans	EA	7	\$20.00	\$140
	5	Install 7 New Trash & Recycling Cans	EA	7	\$1,650.00	\$11,550
	6	Bike Rack	EA	3	\$650.00	\$1,950
	7	Bollard	EA	8	\$1,700.00	\$13,600
	8	Tree Grates Along Depot at Main Street	EA	7	\$1,500.00	\$10,500
	9	Publication Rack	EA	2	\$12,000.00	\$24,000
	10	Interpretive Signage	EA	3	\$4,500.00	\$13,500
Irrigation System	1	Repair/ Expand Irrigation System/ Include in Smart System	LS	1	\$5,000.00	\$5,000
Landscaping	1	Finish Grading	SF	3084	\$0.05	\$154
	2	Soil Amendment	SF	3084	\$0.20	\$617
	3	Shrub/ Groundcover Planting (Including Improvements)	SF	3084	\$2.75	\$8,481
	4	15 gallon Tree	EA	4	\$150.00	\$600
	5	24" Box Trees	EA	8	\$300.00	\$2,400
	6	Bark Mulch 3" Depth	CY	5	\$40.00	\$200
	7	Tree Root Barrier	LF	255	\$18.00	\$4,590
	8	90-Day Maintenance	LS	1	\$13,000.00	\$13,000
					<b>SUBTOTAL</b>	<b>\$312,782</b>
					15% PS & E	\$46,917
					15% Contingency	\$46,917
					<b>PHASE 2 TOTAL</b>	<b>\$406,616</b>



**Colfax Historic Downtown Master Plan**  
**Preliminary Statement of Probable Improvement Cost PHASE 3**  
**North Main Street**  
10/29/2009

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CATEGORY	ITEM	DESCRIPTION	UNIT	QUANT	UNIT PRICE	AMOUNT
Site Preparation	1	Site Layout and Survey	LS	1	\$2,500.00	\$2,500
	2	Construction Staging & Mobilization	LS	1	\$20,000.00	\$20,000
	2	Erosion Control	LS	1	\$400.00	\$400
	3	Temporary Fencing/ Including Tree Protection	LS	1	\$2,000.00	\$2,000
	4	Asphalt Concrete Grinding (2")	SF	15348	\$1.00	\$15,348
	5	Traffic Control	LF	500	\$4.00	\$2,000
Site Work	1	Standard Concrete Paving (New Sidewalk W Side Main Street)	SF	450	\$5.00	\$2,250
	2	Enhanced Paving Crosswalks & Roundabout (Colored)	SF	1025	\$12.50	\$12,813
	3	Asphalt Paving/ Including Class 2 Ag. Base Bank Parking Expansion	SF	1453	\$4.85	\$7,047
	4	Concrete Steps to Bank Parking Lot Expansion	LS	1	\$7,500.00	\$7,500
	5	Concrete Parking Stop Bar	EA	17	\$185.00	\$3,145
	6	Striping	LF	400	\$9.50	\$3,800
	7	Curb and Gutter	LF	100	\$12.00	\$1,200
	1	Remove Existing Trash Cans	EA	7	\$20.00	\$140
	2	Install 8 New Trash & Recycling Cans	EA	8	\$1,650.00	\$13,200
	3	Bike Rack	EA	2	\$650.00	\$1,300
	4	Art Element w/ Concrete Pedestal	EA	1	\$20,000.00	\$20,000
	5	Interpretive Signage	EA	3	\$4,500.00	\$13,500
Irrigation System	1	Repair/ Expand Irrigation System/ Include in Smart System	LS	1	\$2,000.00	\$2,000
Landscaping	1	Finish Grading	SF	1500	\$0.05	\$75
	2	Soil Amendment	SF	1500	\$0.20	\$300
	3	Shrub/ Groundcover Planting	SF	1500	\$2.75	\$4,125
	5	24" Box Trees	EA	1	\$300.00	\$300
	6	Bark Mulch 3" Depth	CY	5	\$40.00	\$200
	7	Tree Root Barrier	LF	20	\$18.00	\$360
	8	90-Day Maintenance	LS	1	\$5,000.00	\$5,000
					<b>SUBTOTAL</b>	<b>\$140,503</b>
					15% PS & E	\$21,075
					15% Contingency	\$21,075
					<b>PHASE 3 TOTAL</b>	<b>\$182,653</b>

**Colfax Historic Downtown Master Plan**  
**Preliminary Statement of Probable Improvement Cost PHASE 4**  
**Depot Plaza and Parking**  
10/29/2009

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CATEGORY	ITEM	DESCRIPTION	UNIT	QUANT	UNIT PRICE	AMOUNT
Site Preparation	1	Site Layout and Survey	LS	1	\$10,000.00	\$10,000
	2	Construction Staging & Mobilization	LS	1	\$50,000.00	\$50,000
	3	Concrete & Asphalt Demolition & Removal	SF	10000	\$4.50	\$45,000
	4	Roadway Excavation and Grading	CY	120	\$75.00	\$9,000
	5	Temporary Fencing/ Including Tree Protection	LS	1	\$4,000.00	\$4,000
	6	Erosion Control	LS	1	\$1,000.00	\$1,000
	7	Traffic Control	LS	1	\$2,000.00	\$2,000
Infrastructure System	1	Sleeving 4" to Median	LS	1	\$2,000.00	\$2,000
	2	Domestic Water Adjustment	LS	1	\$8,000.00	\$8,000
Site Work	1	Standard Concrete Paving	SF	10000	\$5.00	\$50,000
	2	Enhanced Paving (Colored) Cross Walk	SF	6700	\$12.50	\$83,750
	3	Asphalt Paving/ Including Class 2 Ag. Base	SF	413	\$4.85	\$2,003
	4	Street Paving/ Re-surfacing Asphalt Parking Area	LS	6665	\$3.75	\$24,994
	5	Concrete Parking Stop Bar	EA	23	\$185.00	\$4,255
	6	Striping	LF	1000	\$9.50	\$9,500
	7	Curb and Gutter	LF	2000	\$12.00	\$24,000
Amenities and Site Furnishings	1	Relocate/ Adjust Light Fixture	LS	1	\$2,500.00	\$2,500
	2	Interpretive Signage at Plaza	EA	3	\$2,000.00	\$6,000
	3	Historic Train Engine Display w/ Rails and Viewing Platform	LS	1	\$65,000.00	\$65,000
	4	Covered Train Platform & Access Route West of Train Tracks	LS	1	\$120,000.00	\$120,000
	5	Civic Use Area w/ Parking Facility	LS	1	\$225,000.00	\$225,000
Irrigation System	1	Irrigation System (New Planting Areas)	SF	3030	\$1.65	\$5,000
	2	Existing Irrigation System Upgrade/ Include in Smart System	LS	1	\$1,000.00	\$1,000
Landscaping	1	Finish Grading	SF	3030	\$0.05	\$152
	2	Soil Amendment	SF	3030	\$0.20	\$606
	3	Shrub/ Groundcover Planting	SF	3030	\$2.75	\$8,333
	4	15 gallon Tree	EA	7	\$150.00	\$1,050
	5	24" Box Trees	EA	2	\$300.00	\$600
	6	Bark Mulch 3" Depth	CY	5	\$40.00	\$200
	7	Tree Root Barrier	LF	194	\$18.00	\$3,492
	8	Grass Hydroseeding at Frontage Areas	SF	1000	\$0.25	\$250
	9	Landscape Improvements at Station Along Plaza	LS	1	\$10,000.00	\$10,000
	10	90-Day Maintenance	LS	1	\$13,000.00	\$13,000
					<b>SUBTOTAL</b>	<b>\$791,683</b>
					15% PS & E	\$118,752
					15% Contingency	\$118,752
					<b>PHASE 4 TOTAL</b>	<b>\$1,029,188</b>

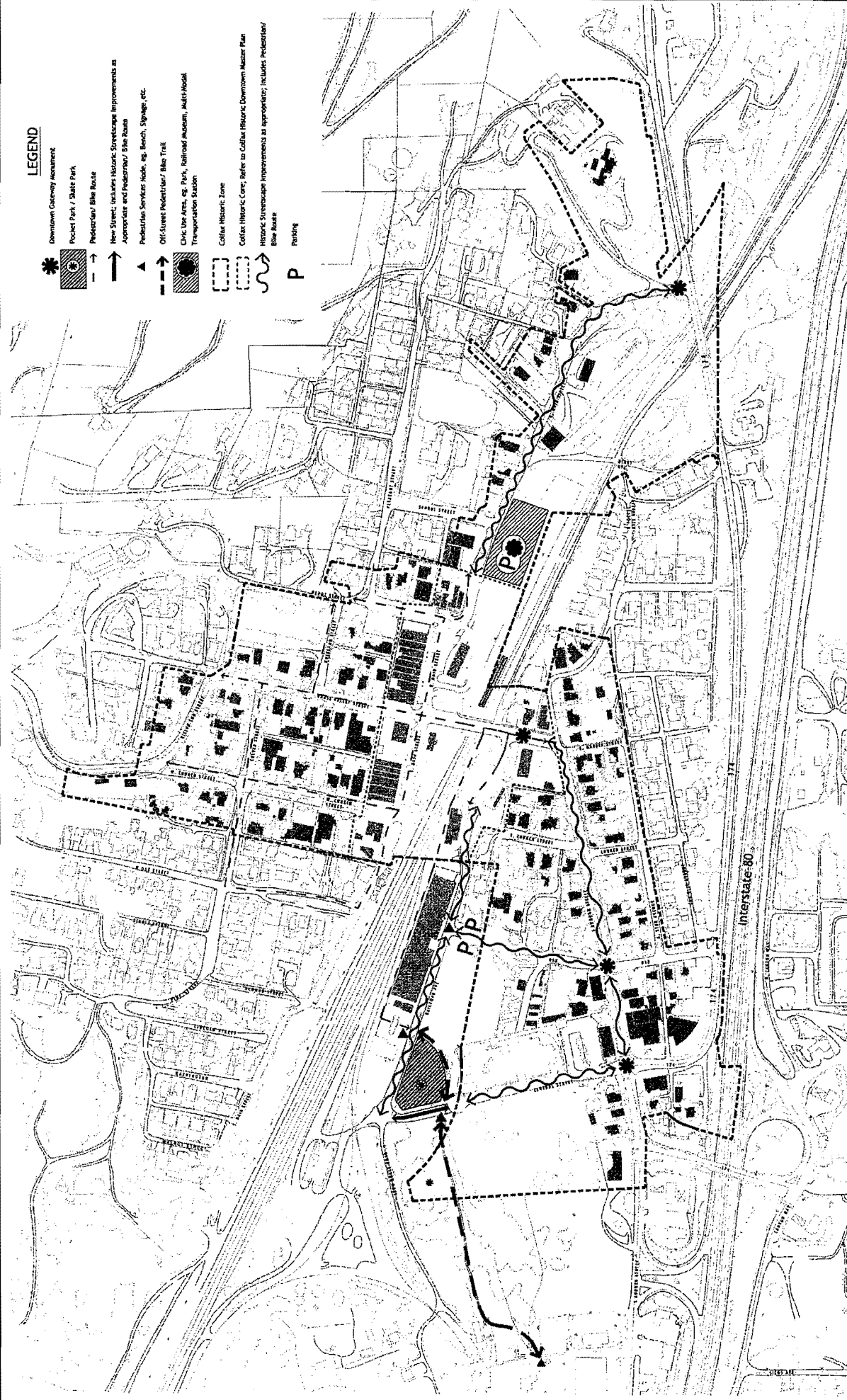
**Colfax Historic Downtown Master Plan**  
**Preliminary Statement of Probable Improvement Cost PHASE 5**  
**Main Street**  
**Library Pocket Park and Parking**  
10/29/2009

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CATEGORY	ITEM	DESCRIPTION	UNIT	QUANT	UNIT PRICE	AMOUNT
Site Preparation	1	Site Layout and Survey	LS	1	\$5,500.00	\$5,500
	2	Construction Staging & Mobilization	LS	1	\$15,000.00	\$15,000
	3	Erosion Control	LS	1	\$1,400.00	\$1,400
	4	Temporary Fencing/ Including Tree Protection	LS	1	\$3,600.00	\$3,600
	5	Traffic Control	LF	750	\$4.00	\$3,000
Infrastructure System	1	Enhanced Paving Crosswalks (Colored)	SF	250	\$12.50	\$3,125
	2	Extended Curb at North Side of Pocket Park	LF	15	\$9.50	\$143
	3	Curb and Gutter at New Parking Area	LF	50	\$12.00	\$600
Site Work	1	Standard Concrete Paving (New Sidewalk W Side Main Street)	SF	450	\$5.00	\$2,250
	2	Wall at Additional Parking Area	LF	150	\$250.00	\$37,500
	3	Wall Along New Sidewalk	LF	160	\$150.00	\$24,000
	4	Asphalt Paving/ Including Class 2 Ag. Parking Expansion	SF	186	\$4.85	\$902
	5	Striping	LF	400	\$9.50	\$3,800
	7	Curb and Gutter	LF	100	\$12.00	\$1,200
Amenities and Site Furnishings	1	Repaint Existing Arbor	LS	1	\$750.00	\$750
	2	Re-Paint Existing Drinking Fountain	EA	1	\$75.00	\$75
	3	Clean Existing Picnic Benches	EA	3	\$100.00	\$300
	4	Landscape Planters w/ Planting	EA	10	\$1,850.00	\$18,500
	5	Art Element w/ Concrete Pedestal	EA	1	\$20,000.00	\$20,000
	7	Landscape Improvements	LS	1	\$1,000.00	\$1,000
	8	Clean Paving and Park Edge Hardscape	LS	1	\$500.00	\$500
Irrigation System	1	Repair/ Expand Irrigation System/ Include in Smart System	LS	1	\$500.00	\$500
					<b>SUBTOTAL</b>	<b>\$143,645</b>
					15% PS & E	\$21,547
					15% Contingency	\$21,547
					<b>PHASE 5 TOTAL</b>	<b>\$186,738</b>

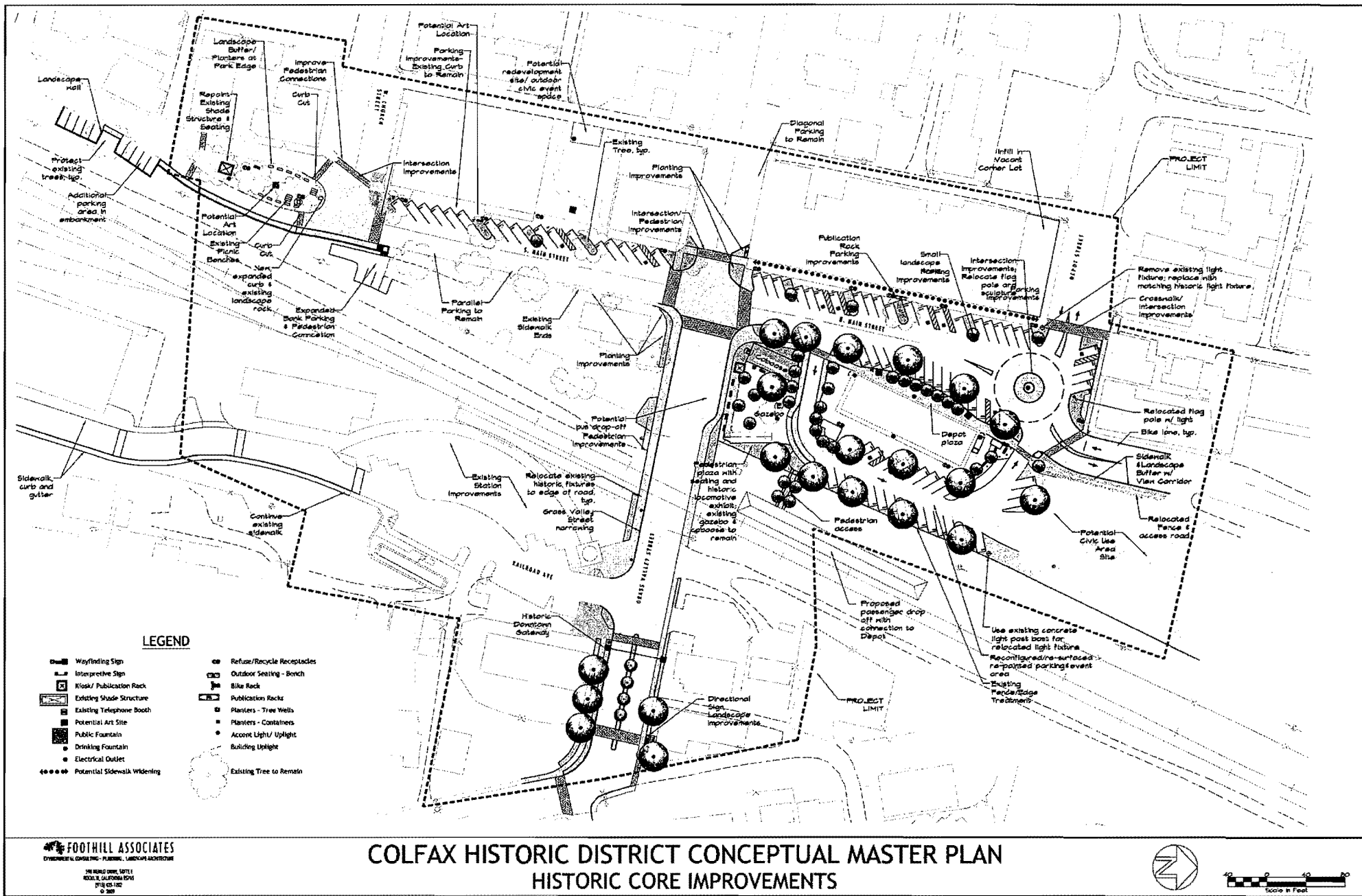
# LEGEND

- Downarrow Gateway Monument
- Product Park / Skate Park
- Pedestrian / Bike Route
- New Street; includes Historic Streetscape Improvements as Appropriate and Pedestrian / Bike Route
- Pedestrian Services; includes Bench, Signage, etc.
- Off-Street Pedestrian / Bike Trail
- City Use Area; includes Park, Railroad Museum, Multi-Modal Transportation Station
- Collar Historic Zone
- Collar Historic Core; Refer to Collar Historic Downtown Master Plan
- Historic Streetscape Improvements as appropriate; includes Pedestrian / Bike Route
- Pending



## COLFAX HISTORIC DISTRICT CONCEPTUAL MASTER PLAN DISTRICT GATEWAYS & CONNECTIONS





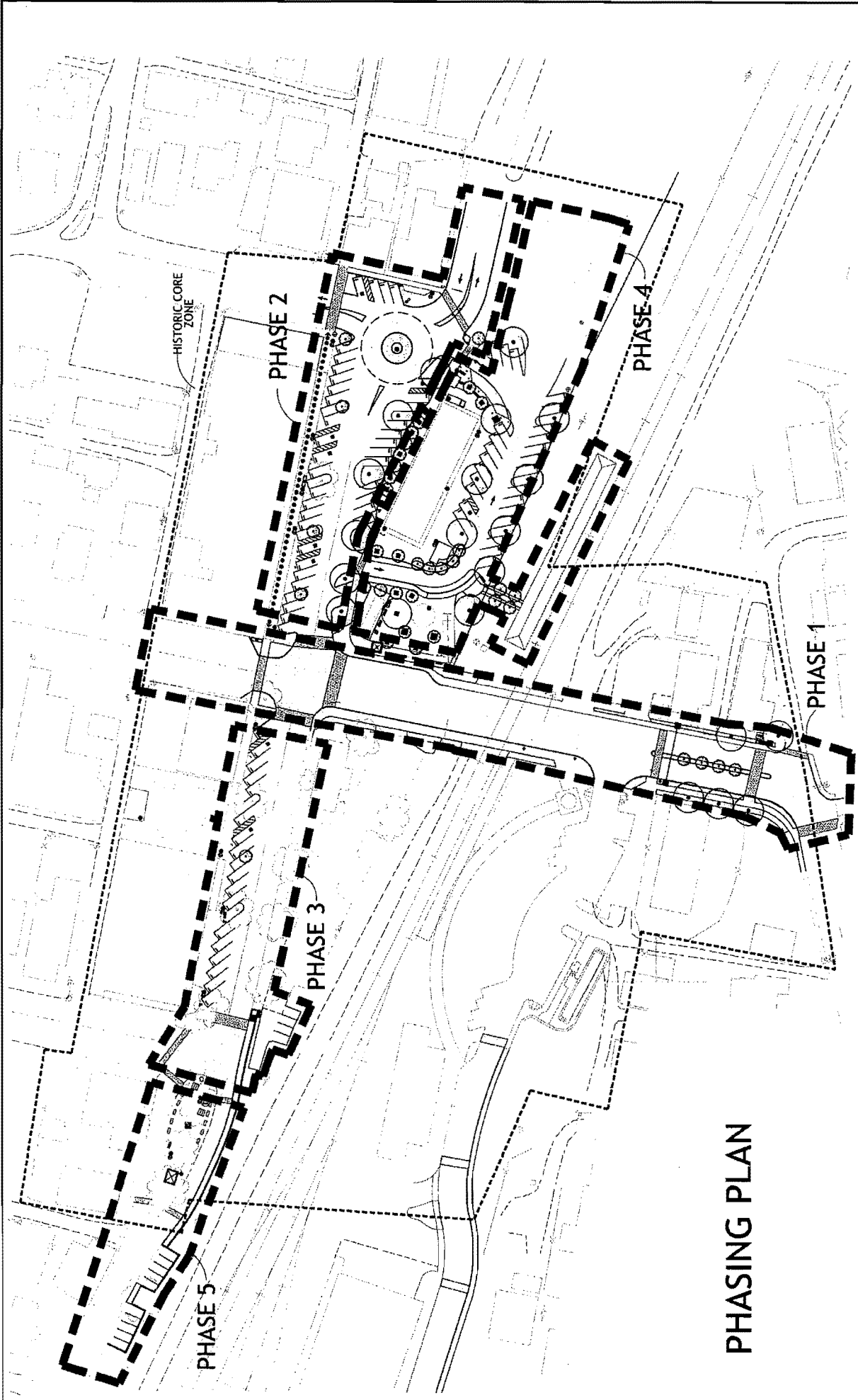
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# PHASING PLAN

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 DENVER, CO 80231  
 303.755.1100

## COLFAX HISTORIC DOWNTOWN CONCEPTUAL MASTER PLAN

